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PLANNING COMMITTEE

Tuesday, 2nd February, 2021 at 7.30 pm

Contact: Jane Creer / Metin Halil
Committee Administrator
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PLEASE NOTE : VIRTUAL MEETING

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Please click [HERE](#) to view the meeting or copy and paste the link below into your web browser:

<https://bit.ly/2Y9X34i>

MEMBERS

Councillors : Maria Alexandrou, Kate Anolue, Mahym Bedekova (Vice-Chair), Sinan Boztas (Chair), Elif Erbil, Ahmet Hasan, Michael Rye OBE, Jim Steven, Hass Yusuf, Susan Erbil, Doug Taylor and Daniel Anderson

N.B. Involved parties may request to make a deputation to the Committee by contacting Democracy@enfield.gov.uk before 10am on the meeting date latest

AGENDA – PART 1

1. WELCOME AND APOLOGIES FOR ABSENCE
2. DECLARATION OF INTEREST
3. MINUTES OF THE PLANNING COMMITTEE HELD ON TUESDAY 5 JANUARY 2021 (Pages 1 - 6)

To receive the minutes of the planning committees held on Tuesday 5 January 2021.

4. REPORT OF THE HEAD OF PLANNING (Pages 7 - 8)

To receive the covering report of the Head of Planning.

5. 20/02461/CAAD - OAKWOOD LODGE, AVENUE ROAD, LONDON, N14 4DE (THE "SITE") (Pages 9 - 52)

RECOMMENDATION: That a positive Certificate be issued under section 17(1)(a) of Part III of the Land Compensation Act 1961, as amended by Part 9 of the Localism Act 2011, indicating that in the Local Planning Authority's opinion there is development, for the purposes of section 14 of the Land Compensation Act 1961 that is appropriate in relation to the acquisition and that planning permission would have been granted, subject to the conditions detailed below in this report, for development comprising of:

- 2x 1bed self-contained units (1 unit within each roof space) and erection of dormer

windows and skylights.

WARD: Cockfosters

6. 20/03070/FUL - FALCON ROAD SPUR, EN3 4LX (Pages 53 - 74)

RECOMMENDATION: That subject to the completion of a Deed of Variation to the Section 106 Agreement, the Head of Development Management / Planning Decisions Manager be authorised to Grant planning permission subject to conditions.

WARD: Ponders End

7. 20/03404/FUL - CHASE FARM HOSPITAL, THE RIDGEWAY, EN2 8JL (Pages 75 - 98)

RECOMMENDATION: That planning permission be Granted subject to conditions.

WARD: Highlands

8. FUTURE MEETING DATES

The next meeting of the Planning Committee will be 23 February 2021.

PLANNING COMMITTEE - 5.1.2021

**MINUTES OF THE MEETING OF THE PLANNING COMMITTEE
HELD ON TUESDAY, 5 JANUARY 2021****COUNCILLORS**

PRESENT Maria Alexandrou, Kate Anolue, Mahym Bedekova, Sinan Boztas, Elif Erbil, Ahmet Hasan, Michael Rye OBE, Jim Steven, Hass Yusuf, Susan Erbil, Ergin Erbil and Derek Levy

ABSENT

OFFICERS: Andy Higham (Head of Development Management), Allison de Marco (Planning Decisions Manager - Strategic Applications), Sharon Davidson (Planning Decisions Manager), Joseph McKee (Senior Planning Officer), Mike Hoyland (Senior Transport Planner), John Hood (Legal Services), Sarah Cary (Executive Director Place), Dominic Millen (Group Leader Transportation) and Jeremy Chambers (Director of Law and Governance) Jane Creer (Secretary) and Metin Halil (Secretary)

Also Attending: Members of the public, applicant and agent representatives were able to observe the meeting live online.
Councillor Daniel Anderson.

1**WELCOME AND APOLOGIES FOR ABSENCE**

NOTED

1. Councillor Boztas (Chair) welcomed all attendees to the meeting, which was being broadcast live online. Committee members confirmed their presence and that they were able to hear and see the proceedings.
2. There were no apologies for absence.
3. Councillor Ergin Erbil substituted for Councillor Doug Taylor.
4. Councillor Derek Levy substituted for Councillor Daniel Anderson.

2**DECLARATION OF INTEREST**

NOTED

1. There were no declarations of interest.

3**MINUTES OF THE PLANNING COMMITTEE HELD ON TUESDAY 17
NOVEMBER 2020 AND TUESDAY 24 NOVEMBER 2020**

PLANNING COMMITTEE - 5.1.2021

NOTED

1. The minutes of the Planning Committee meeting held on Tuesday 17 November 2020 and Tuesday 24 November 2020 were agreed.

**4
REPORT OF THE HEAD OF PLANNING**

RECEIVED the report of the Head of Planning.

**5
ORDER OF THE AGENDA**

AGREED to vary the order of the agenda. The minutes follow the order of the meeting.

**6
20/02475/FUL AND 20/02476/ADV - MERIDIAN STUDIOS, HAWLEY ROAD,
LONDON, N18 3QU**

NOTED

1. The introduction by Joseph McKee, Senior Planning Officer, clarifying the proposals.
2. Members debate and questions responded to by officers.
3. The support of the majority of the committee with 11 votes for and 1 against.

AGREED that temporary planning permission and advertising consent be granted subject to the conditions for a period of 15 years.

**7
20/01049/FUL AND ASSOCIATED LISTED BUILDING CONSENT
20/01188/LBC - CAR PARK ADJACENT TO ARNOS GROVE STATION,
BOWES ROAD, LONDON, N11 1AN**

NOTED

1. The introduction by Allison De Marco, Planning Decisions Manager, clarifying the proposals.
2. An update report had been circulated to Members, including additional representations received since the Main Report was published. Officers considered no substantial new issues had arisen but given the length of the representations, and for clarity, Officers had responded to objections by referring to sections in the Main Report that assessed the matters raised. A verbal update on additional representations: a petition had been re-circulated by Bowes Road residents group, this petition has been

PLANNING COMMITTEE - 5.1.2021

- previously circulated to members and considered and address in the Main Report at Section 6; and no objection from the twentieth century society.
3. The Planning Decision Manager explained the applications under consideration had been listed on the agenda for consideration on 24 November 2020 and had been withdrawn from consideration by officers. Officers reviewed representations received in the lead-up to the 24 November 2020 and several omissions. Officers were confident the issues raised in the representations had been fully considered.
 4. The deputation of Henry Grala (Local resident) speaking against the officer's recommendation.
 5. The deputation of Peter Gibbs (Federation of Enfield Residents & Allied Associations) speaking against the officer's recommendation.
 6. The deputation of Virginia Knox (Local resident) speaking against the officer's recommendation.
 7. The statement of the Rt Hon Theresa Villiers MP (Chipping Barnet).
 8. The statement of Councillor Roberto Weeden-Sanz, Brunswick Park Ward (LB of Barnet).
 9. The statement of Councillor Daniel Anderson, Southgate Green Ward Councillor.
 10. The response of Lee Campbell (Transport for London) on behalf of the applicant.
 11. The response of Richard Lavington (Scheme Architect) on behalf of the applicant.
 12. The response of Rebecca Crow (Grainger PLC) on behalf of the applicant.
 13. Members lengthy debate and questions responded to by officers.
 14. Officers' noted the committee's concerns and comments:
 - Arnos Grove car park is popular with local residents due to the regularity of the Piccadilly line service which provides a level of safety for single people travelling late at night. There is an issue here in terms of meeting the standards of the Equality Act.
 - The application involves a Grade 2 listed building and block BD 01 (east side) obscures views of the drum.
 - Both sides of the car park are small and this was an overdevelopment which is to be crammed into the space. The intended blocks, ranging from 4 to 7 storeys in height would impact Arnos Park, the surrounding area, the listed arches and would change the character of the area.
 - The social housing is not affordable to people of an average income in Enfield. Concern that the proposed secure tenancy of 1 – 5 years would not be meeting the needs of those less well off and vulnerable in the borough.
 - Point 6.62 of the report showed that the needs of new people, in terms of health provision is being overlooked. There was nothing in then report to tackle additional medical services.
 - The proposed new drop off point had not been adequately addressed in the report. How would the proposed drop off area work for people situated at the end of the development in terms of deliveries from companies like Amazon. The proposed new drop off point would increase more car journeys with the loss of the car park.

PLANNING COMMITTEE - 5.1.2021

- Request that an extra condition be added to remove the non-compliance Cycle Enfield store outside the listed building and be re-located.
 - No detail in the report about the bus stand and recognised issue. It would be resolved in the future, but no solutions given in the report.
 - Bowes Road had a major issue with traffic accidents. There had been no improvements and the report had not been conditioned to show how this would be managed.
 - How many apprenticeships and schemes of work into education and training opportunities would the scheme provide for local young people in the borough including the number for ethnic minorities.
 - In terms of sustainability of the project, would the development involve environment friendly methods of construction, materials sourced locally, provide vertical gardens and any radical solutions to provide green spaces.
 - There were too many 1 and 2-bedroom units and not enough family units of 3 bedrooms. Issue with segregation in terms of the A02 building. All the units in the A02 building should be affordable housing.
 - The scheme did not consider Enfield's housing needs by the lack of 3-bedroom units.
 - The loss of the car park would cause problems for key workers, people going to work and severely impact the elderly.
 - Residents views ignored in light of a 3,000 signatory petition. Road closures in Bowes Road and Winchmore Hill has caused more traffic congestion and made roads dangerous. The majority of TFL stations did not have car parks in central London but in the suburbs a car is needed.
15. The Chair requested a response from Andy Higham, Head of Development Management. The Head of Development Management explained that Officers took these concerns and comments very seriously and had taken the issues raised during consideration of the applications very seriously. In writing their report, Officers had fully considered issues that had been raised, including those raised by objectors. While Members may have differing views on the conclusions, Officers had fully considered objections received and issues raised alongside the merits of the proposal.
16. During the debate, it was **AGREED** that the rules of procedure within the Council's Constitution relating to the time meetings should end (10pm) be suspended for a period of 45 minutes to enable the business of the agenda to be completed.
17. During the debate, it was **AGREED** that the rules of procedure within the Council's Constitution relating to the time meetings should end be suspended for a further period of 15 minutes to enable the business of the agenda to be completed.
18. The majority of the committee did not support the officers recommendation: 11 votes against and 1 abstention'.
19. The reasons for refusal were discussed and agreed:

PLANNING COMMITTEE - 5.1.2021

- **Loss of Car Park and Implications on Surrounding Highway network** -The proposed development would not adequately mitigate the loss of existing parking for the station leading to increased pressure in the surrounding area and circumstances detrimental to safety, security and the use of the station by local residents contrary to Policy 26 of the Enfield Core Strategy and Policy 45 of the Enfield Development Management Document.
- **Effect on Building (B01) on setting of Listed Building** - The proposed development, due to the siting and scale of building B01 relative to the road frontage, would fail to preserve or enhance the setting of the Grade II* listed Arnos Grove Underground Station and associated structures. This would be contrary to Policy DMD 44 of the Enfield Development Management Document, Policy CP31 of the Enfield Core Strategy and Policy 7.8 of the adopted London Plan.
- **Insufficient Family Housing** - The proposed development fails to provide an appropriate composition of housing (mix / tenure / rent levels) to meet local housing needs, including the need for genuinely affordable and family housing in the Borough. It would fail to provide a range of housing choice, fail to assist in achieving a mixed and balanced community and constitute unsustainable development contrary to the National Planning Policy Framework (2019), Policies CP3 and CP5 of the of the Enfield Core Strategy, Policies DMD 1 and DMD3 of the Enfield Development Management Document, Policies 3.8, 3.9, 3.11 and 3.12 of the adopted London Plan and Policies GG4, H6, H11 and H10 of the Intend to Publish London Plan and the Mayor of London Affordable Housing and Viability SPG.

20. The unanimous support of the committee for the reasons given for refusal.

AGREED that the application be Refused for the reasons given.

8

FUTURE MEETING DATES

NOTED

1. The next meeting dates of the planning committee will be as follows:
 - 19 January 2021
 - 2 February 2021 –
Post meeting note - This provisional meeting was agreed after the 5 January 2021 committee meeting.

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MUNICIPAL YEAR 2020/21

COMMITTEE:
 PLANNING COMMITTEE
 02.02.2021

AGENDA - PART 1	ITEM 4
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SUBJECT -

MISCELLANEOUS MATTERS

REPORT OF:
 Head of Planning

Contact Officer:
 Planning Decisions Manager
 David Gittens Tel: 020 8379 8074
 Claire Williams Tel: 020 8379 4372

3.1 PLANNING APPLICATIONS AND APPLICATIONS TO DISPLAY ADVERTISEMENTS

On the Schedules attached to this report I set out my recommendations in respect of planning applications and applications to display advertisements. I also set out in respect of each application a summary of any representations received and any later observations will be reported verbally at your meeting.

Background Papers

- (1) Section 70 of the Town and Country Planning Act 1990 states that the Local Planning Authority shall have regard to the provisions of the development plan, so far as material to the application, and to any other material considerations. Section 54A of that Act, as inserted by the Planning and Compensation Act 1991, states that where in making any determination under the Planning Acts, regard is to be had to the development, the determination shall be made in accordance with the plan unless the material considerations indicate otherwise. The development plan for the London Borough of Enfield is the London Plan (March 2015), the Core Strategy (2010) and the Development Management Document (2014) together with other supplementary documents identified in the individual reports.
- (2) Other background papers are those contained within the file, the reference number of which is given in the heading to each application.

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LONDON BOROUGH OF ENFIELD

PLANNING COMMITTEE

Date: 2 February 2021

Report of
Head Of Planning

Contact Officer:
Andy Higham
Sharon Davidson
Joe Aggar
Tel No: 0208 132 0878

Ward:
Cockfosters

Application Number: 20/02461/CAAD

Category: Other

LOCATION: Oakwood Lodge, Avenue Road, London, N14 4DE (the "Site")

PROPOSAL: Conversion of existing roof space of each of the two residential blocks to provide a total of 4 x 1-bed self-contained flats and erection of dormer windows, skylights and alteration to external staircase (Application A) and;
Conversion of existing roof space of each of the two residential blocks to provide a total of 2 x 2-bed self-contained flats and erection of dormer windows and skylights (Application B).

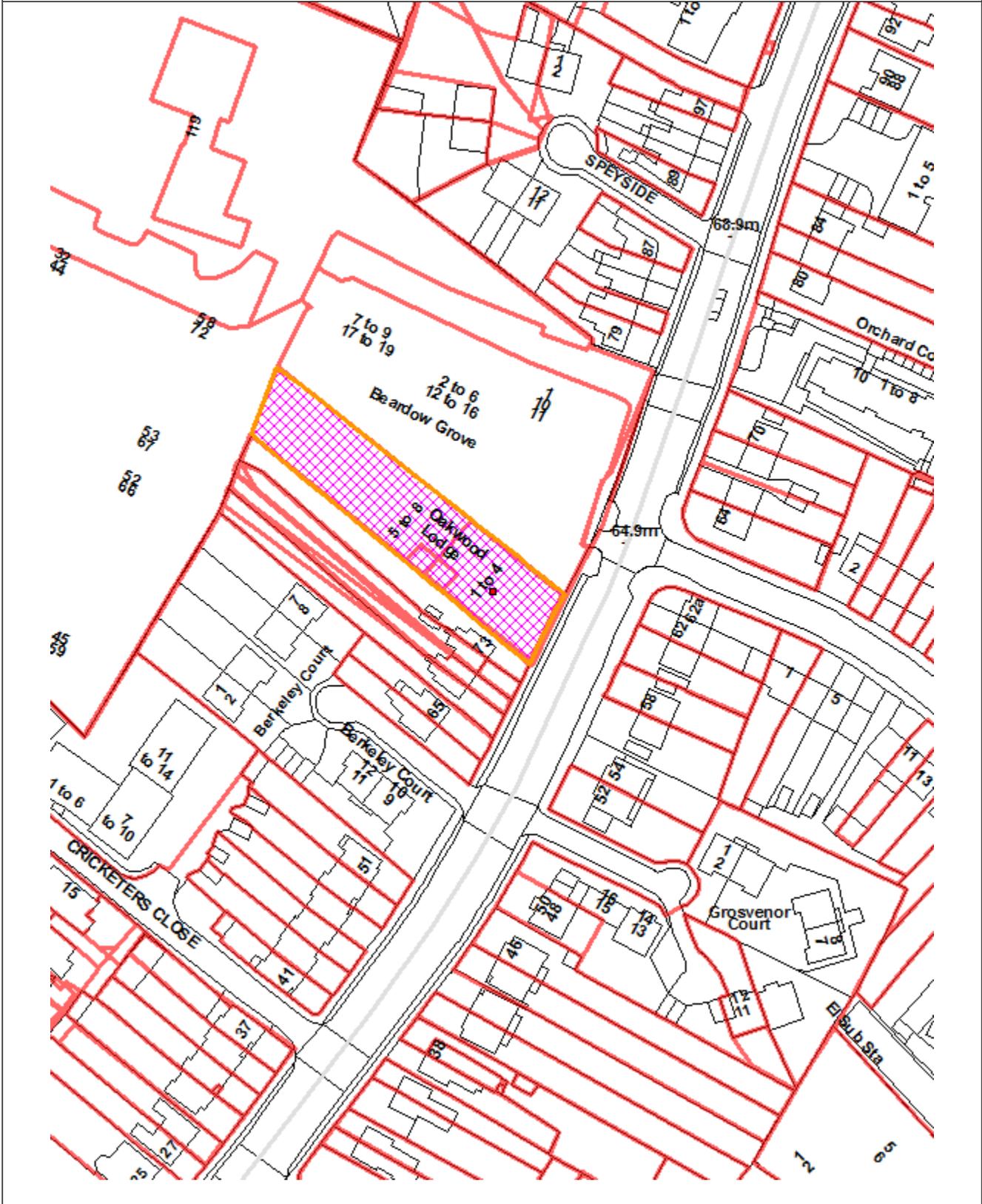
Applicant Name & Address:
c/o Grandpex Company Ltd

Agent Name & Address:
Keith Murray Consultants
Magdalen House
80 High Street
Bushey
Herts
WD23 3HD

RECOMMENDATION: That a positive Certificate be issued under section 17(1)(a) of Part III of the Land Compensation Act 1961, as amended by Part 9 of the Localism Act 2011, indicating that in the Local Planning Authority's opinion there is development, for the purposes of section 14 of the Land Compensation Act 1961 that is appropriate in relation to the acquisition and that planning permission would have been granted, subject to the conditions detailed below in this report, for development comprising of:

- 2x 1bed self-contained units (1 unit within each roof space) and erection of dormer windows and skylights.

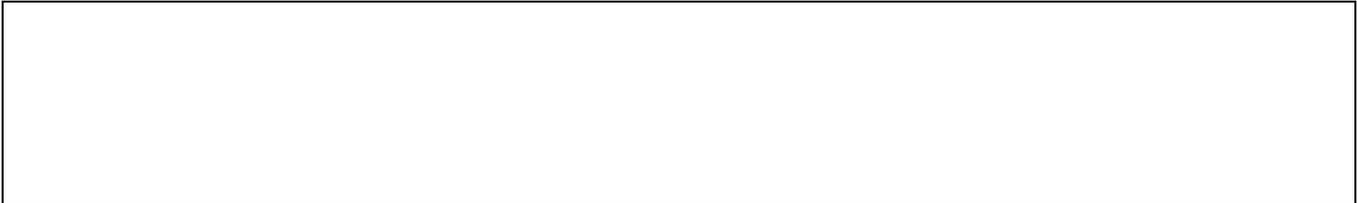
Ref: 20/02461/CAAD LOCATION: Oakwood Lodge , Avenue Road, London, N14 4DE



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1. Note for Members and Background

1.1. The application is reported to planning committee as Officers do not have Delegated Authority to issue a decision.

1.2. Planning permission was granted for the redevelopment of the Site and adjoining land under application reference 16/01578/FUL in 2018. The construction of the development pursuant to reference 16/01578/FUL involved the demolition of the buildings on this Site which was acquired by the London Borough of Enfield pursuant to a compulsory purchase order in 2017. Works are underway in relation to Phase 1 of planning permission 16/01578/FUL although the redevelopment of the Site has now been completed (referred to as Block F1 in planning approval 16/01578/FUL).



Image 1 – Site Location Plan (20/02461/CAAD)



Image 2 – Site Location Plan (16/01578/FUL)

These applications are made under Section 17 of the Land Compensation Act 1961 (amended by Section 63 of the Planning and Compensation Act 1991) and seek a Certificate of Appropriate Alternative Development (CAAD) to establish what planning permissions, if any, would have been granted had the Site not been acquired compulsorily.

A CAAD is in effect a hypothetical planning permission provided solely for valuation purposes when a public authority acquires land compulsorily. The purpose of a certificate is to assist in the assessment of the open market value of the Site. It is therefore necessary to consider whether, as at the relevant valuation date (RVD) planning permission could reasonably have been expected to be granted for the development proposed within these applications, or for any other form of development, in the circumstances known to the market at that time, on the assumption that the regeneration scheme underpinning the Compulsory Purchase Order (CPO) had been cancelled.

The relevant valuation date (RVD) is 23 November 2017, which is when the Council as landowner took possession of the Site.

Section 17(5) of the Land Compensation Act 1961 provides that where a certificate is issued under subsection (1) (a) it must:

- (a) identify every description of development that in the local planning**

authority's opinion would be appropriate alternative development; and

- (b) give a general indication –
 - (i) of conditions to which planning permission for the development could reasonably have been expected to be subject.
 - (ii) of when permission could reasonably have been expected to be granted only at a time after the relevant valuation date, and
 - (iii) of any pre-condition for granting the permission (for example, entry into an obligation) that could reasonably have been expected to have to be met.

Guidance suggests that an LPA should seek to come to a view, based on its assessment of the information contained within the application and of the policy context applicable at the relevant valuation date, the character of the Site and its surroundings, as to whether such a development would have been acceptable to the Authority. As the developments included in the certificate are not intended to be built the local planning authority does not need to concern itself with whether or not the granting of a certificate would create any precedent for the determination of future planning applications.

2. Proposal

The application is seeking to establish if planning permission would have been granted for two proposals. The first application (referred to as Application A) is for the conversion of the roof space of each block to create 4 x 1-bedroom units (2x1 bedroom units in each roof space). To facilitate the conversion, associated works are proposed in the form of hip to gable roof extensions, the construction of front and rear dormers at roof level, plus the reconfiguration of the external staircase, to facilitate access to the upper floor flats.

A second submission (referred to as Application B) for an alternative proposal has also been submitted. This proposal seeks to establish if planning permission would have been granted for the alteration of the roof space of each block to create 2 x 2-bedroom units (1 unit in each roof space). To facilitate the conversion, the proposal includes the constructions of dormers, reconfiguration of the access stairs internally and inclusion of rooflights.

The LPA must be satisfied on the balance of probabilities that planning permission would have been granted for the description of development applied for and it does not have to assess more precisely the prospects of that development happening or of the permission being implemented

LPAs may issue a certificate for other uses or conclude that there is no development for which permission would have been given. Conditions that would have been applicable must be specified along with any other obligations that could reasonably have been expected to be imposed. The CAAD is intended to assist the compensation assessment by clarifying what the maximum development potential of the site was at the RVD.

3. Site and Surroundings at the date of the RVD

For the purposes of this application, the Site previously comprised two detached buildings each occupied by 4 x 2 bed flats (8 x self-contained units in total on the Site).

Photos of Site/Street



Image 3 – Aerial of Oakwood Lodge

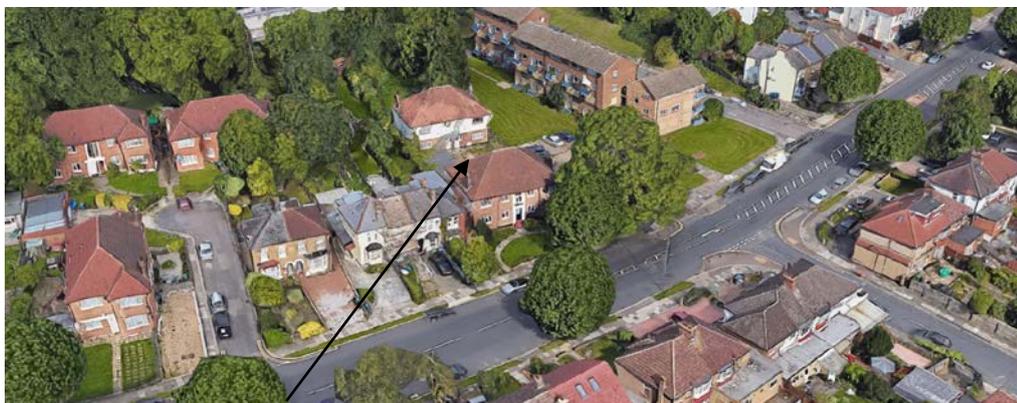


Image 4 – street overview to front elevation of Oakwood Lodge from Hood Avenue

- 3.2. The buildings faced away from each other, separated by a shared parking court and garages. One flatted block was sited fronting Avenue Road, slightly set back, with a front garden area. The second flatted block was sited in parallel but set well into the Site with the main elevation facing the rear gardens. The parking court had a block of four garages. Pedestrian access to the rear block is assumed to have been shared with the vehicle access, but then extended past the building to wrap round to the main entrance on the rear facing elevation.

The buildings were two storeys in height, each with a hip end roof. The main access to the upper floor flats (as originally designed) would have been from an internal communal staircase, although an external staircase on the parking court facing elevation of each building existed. Communal grounds surrounded the two flatted blocks.

The Site faced on to Avenue Road which is a long and busy classified road which provides access to many residential streets within a relatively built-up area. Avenue Road drops in height from Chase Road and rises at the junction with Chase Side.

4. Relevant Planning History

Oakwood Lodge

TP/05/1146 Demolition of existing garage block and erection of a 2-storey 2-bed detached house together with 5 car parking spaces. Application Refused and Appeal Dismissed.

Reasons for refusal:

1. *The proposed 2-storey dwelling by reason of its siting design and sub-division of the site, would lead to the infilling of a space, thereby resulting in the introduction of an overly dominant and visually incongruous form of development out of character and appearance, form and pattern of the surrounding area and the visual amenities enjoyed by neighbouring properties. This would be contrary to Policies (I)GD1, (I)GD2 and (II)GD3 of the Unitary Development Plan.*
2. *The proposed 2-storey dwelling by reason of its size and siting would give rise to conditions through a loss of light and outlook, which would adversely affect the residential amenities enjoyed by the occupiers of the ground floor flats. This would be contrary to Policies (I)GD1, (I)GD2 and (II)GD3 of the Unitary Development Plan.*
3. *The proposed dwelling, due to its size, siting and visual prominence, would detract from the outlook enjoyed by the occupiers of the neighbouring property, 73 Avenue Road, detrimental to their amenity and contrary to Policies (I)GD1, (I)GD2 and (II)GD3 of the Unitary Development Plan.*
4. *The proposed two-bed dwelling house due to the open nature of the plot would provide insufficient amenity space having regard to the adopted standards of the Council and would, as a result, lead to the*

creation of an unacceptable form of residential accommodation contrary to Policy (II)H9 of the Unitary Development Plan.

5. The proximity of the proposed parking space to the proposed bedroom No. 2 would result in an unacceptable level of disturbance detrimental to the residential amenities of the proposed occupier of the new dwelling contrary to Policies (I)GD1, (I)GD2 and (II)GD3 of the Unitary Development Plan.

5. Relevant Policies at RVD

London Plan (2016)

Policy 3.3 Increasing housing supply
Policy 3.4 Optimising housing potential
Policy 3.5 Quality and design of housing developments
Policy 3.6 Children and young people's play and informal recreation facilities
Policy 3.8 Housing choice
Policy 3.9 Mixed and balanced communities
Policy 3.13 Affordable housing thresholds
Policy 3.14 Existing housing
Policy 5.1 Climate change mitigation
Policy 5.2 Minimising carbon dioxide emissions
Policy 5.3 Sustainable design and construction
Policy 5.5 Decentralised energy networks
Policy 5.6 Decentralised energy in development proposals
Policy 5.7 Renewable energy
Policy 5.8 Innovative energy technologies
Policy 5.9 Overheating and cooling
Policy 5.10 Urban greening
Policy 5.11 Green roofs and development site environs
Policy 5.13 Sustainable drainage
Policy 5.14 Water quality and wastewater infrastructure
Policy 5.15 Water use and supplies
Policy 5.16 Waste net self-sufficiency
Policy 5.18 Construction, excavation and demolition waste
Policy 5.21 Contaminated land
Policy 6.3 Assessing the effects of development on transport capacity
Policy 6.5 Funding Crossrail and other strategically important infrastructure
Policy 6.9 Cycling
Policy 6.10 Walking
Policy 6.11 Smoothing traffic flow and tackling congestion
Policy 6.12 Road network capacity
Policy 6.13 Parking
Policy 7.1 Lifetime neighbourhoods
Policy 7.2 An inclusive environment
Policy 7.3 Designing out crime
Policy 7.4 Local character
Policy 7.6 Architecture
Policy 7.7 Location and design of tall and large buildings
Policy 7.14 Improving air quality

Policy 7.15 Reducing noise and enhancing soundscapes
Policy 7.18 Protecting open space and addressing deficiency
Policy 7.19 Biodiversity and access to nature
Policy 8.2 Planning obligations
Policy 8.3 Community infrastructure levy

Core Strategy

CP2: Housing supply and locations for new homes
CP4: Housing quality
CP5: Housing types
CP9: Supporting community cohesion
CP13: Promoting economic prosperity
CP16: Taking part in economic success and improving skills
CP18: Delivering shopping provision across Enfield
CP20: Sustainable energy use and energy infrastructure
CP21: Delivering sustainable water supply, drainage and sewerage infrastructure
CP22: Delivering sustainable waste management
CP24: The road network
CP25: Pedestrians and cyclists
CP26: Public transport
CP28: Managing flood risk through development
CP30: Maintaining and improving the quality of the built and open environment
CP31: Built and landscape heritage
CP32: Pollution
CP36: Biodiversity
CP46: Infrastructure contributions

Development Management Document

DMD3 Providing a Mix of Different Sized Homes
DMD4 Loss of Existing Residential Units
DMD6 Residential Character
DMD8 General Standards for New Residential Development
DMD9 Amenity Space
DMD10 Distancing
DMD37 Achieving High Quality Design-Led Development
DMD38 Design Process
DMD43 Tall Buildings
DMD44 Preserving and Enhancing Heritage Assets
DMD45 Parking Standards
DMD47 New Roads, Access and Servicing
DMD48 Transport Assessments
DMD49 Sustainable Design and Construction Statements
DMD50 Environmental Assessment Methods
DMD51 Energy Efficiency Standards
DMD53 Low and Zero Carbon Technology
DMD54 Allowable Solutions
DMD55 Use of Roof Space / Vertical Surfaces
DMD56 Heating and Cooling
DMD57 Responsible Sourcing of Materials
DMD58 Water Efficiency
DMD59 Avoiding and Reducing Flood Risk
DMD60 Assessing Flood Risk
DMD61 Managing Surface Water

DMD65 Air Quality
DMD68 Noise
DMD69 Light Pollution
DMD70 Water Quality
DMD72 Open Space Provision
DMD73 Children's Play Space
DMD78 Nature Conservation
DMD79 Ecological Enhancements
DMD81 Landscaping
DMD70 Water Quality
DMD75 Waterways
DMD77 Green Chains
DMD78 Nature Conservation
DMD79 Ecological Enhancements
DMD80 Trees on Development Sites
DMD81 Landscaping

Other Material Considerations

National Planning Policy Framework 2012 (NPPF)
National Planning Practice Guidance 2016 (NPPG)
A City for All Londoners (2016)

(2017) GLA: Homes for Londoners: Affordable Housing and Viability SPG

GLA: Housing SPG (2016)

GLA: Social Infrastructure SPG (2015)

GLA: The Control of Dust and Emissions during Construction and Demolition SPG (2014)

GLA: London Sustainable Design and Construction SPG (2014)

GLA: Accessible London: Achieving an Inclusive Environment SPG (2014)

(2012) GLA: Shaping Neighbourhoods: Play and Informal Recreation SPG

GLA: Shaping Neighbourhoods: Character and Context SPG (2014)

London Borough of Enfield S106 SPD (2016)

Enfield Characterisation Study (2011)

6. Assessment

The main issues arising from this proposal relate to:

Land Use
Design and Appearance
Neighbouring Amenity
Quality of the accommodation proposed
Unit Mix
Transport
Other Matters

Land Use

This hypothetical assessment proceeds on the basis that the Site, is as it was on 23 November 2017, when it comprised two residential blocks,

containing 8 x 2 bed units. The proposals from the applicant, show alterations to the flatted blocks to allow for the conversion of the roof space to provide 4 x 1-bedroom units (referred to as Application A) and 2 x 2-bedroom units (referred to as Application B).

Both schemes would have led to additional residential unit numbers and considered in line with London Plan 3.4 and Policy CP5 of the Core Strategy, that seek to support increased housing delivery. The land use would remain conducive with the surroundings and in principle support the delivery of new housing. This is subject to other relevant policies within the Local Development Framework, having regard to the Site's characteristics in terms of urban design, quality of accommodation, transport, and neighbouring amenity.

Design and Appearance

The two 'existing' blocks were located in a residential area and comprised two detached, two storey, flatted blocks. The roof was hipped (with no habitable accommodation in the roof space) and two chimneys rising from the flank elevations. The Site occupied a long rectangular plot which fronted onto Avenue Road with trees and vegetation to the rear.

The main facades of the blocks faced away from one another. Between the properties was a shared parking area and single storey garages. Notably the front block faced directly onto Avenue Road and therefore was highly visible in public views and in this respect occupied a prominent location.

The proposal for Application A would involve altering the roof form to create gable ends and adding dormers to the front and rear roof slopes – two sizeable dormers to the 'front' elevations and a single, joined dormer with staircase to the rear elevations to facilitate two new residential units within each block.

The proposal for Application B would involve more modest alterations to the roof, with the insertion of two dormer windows, one on each roof slope. The proposed dormers would be incorporated into the pitched roofs of the two blocks. The dormers would face into the internal courtyard and therefore views of these would be limited from the public domain.

DMD Policy 6 required development to be of a scale and form appropriate to the existing pattern of development or setting, having regard to the character typologies. Further, Policy DMD8 provided that all development must be of an appropriate scale, bulk and massing and be appropriately located having regard to the nature of the surrounding area.

DMD13 required that roof extensions be in keeping with the character of the property, not be dominant when viewed from the surrounding area, and where to the side of a property, must not disrupt the character or balance of the property. Roof dormers on front facing roofs would generally only be permitted if they did not materially affect the character of the area and would not be dominant or intrusive when viewed from the surrounding area.

Finally, consistent with Policy CP30 of the Core Strategy, DMD37 of the Development Management Document and the design-led approach advocated by the NPPF, the suitability of a development had to be measured in part on its overall quality and function to ensure development was appropriately located and had regard to both the subject dwelling and the surrounding area.

Given the changes in land levels to Avenue Road, the host properties were set slightly at a lower level than the adjoining terraced properties. Nevertheless, the flatted blocks on the Site were more substantial in bulk, mass and scale, with the built form projecting deeper than the adjoining terraced buildings. Of note, front dormers were not a feature within the streetscape.



Image 5 -Front elevation to Oakwood Lodge

The proposed alteration to the roof form in Application A would considerably modify and dominate the roof slope to both buildings. The proposed dormers would be a large and incongruous modification, not in keeping with, and harmful to the character and appearance of the host buildings and the surrounding area. Overall, the alterations would have resulted in a highly conspicuous roof form, given their scale and elevated position relative to the adjoining properties.

The combination of the gable extensions and the larger front dormer windows would have significantly increased the bulk and mass of the building directly adjacent to the modest terraced properties. This discordancy would have been exacerbated by the windows to the roof extension which neither match nor align with the fenestration pattern to the lower floors. Viewed in combination, the proposed alterations at roof level, by reason of their size and bulk would have appeared as disproportionate and discordant additions to the original character and appearance of the host property.

Whilst it is acknowledged the two dormers to the rear block and the rear dormer to the front block would have had less of an impact in the public domain. However, as matter of policy, reduction in visibility does not equate to a policy compliant scheme. For the reasons outlined above, the proposal would have resulted in an unacceptable impact to the roof form, host building and surrounding area.

Officers have therefore concluded that the proposed alterations within Application A would have had an unacceptably harmful effect on the character and appearance of the Site and the surrounding area. It would have conflicted with Policies 7.4 and 7.6 of the London Plan 2016, Core Policy 30 of The Enfield Plan Core Strategy 2010 and Policies DMD 8, DMD 13, and DMD 37 of the Enfield Development Management Document (DMD) 2014. Amongst other things, these policies sought to resist development that is inappropriate to its context or which fails to have appropriate regard to its surroundings. The proposal would have also conflicted with the NPPF which sets out that planning should always seek to secure high quality design.

The proposal within Application B involves the insertion of two dormer windows, one on each roof slope. The proposed dormers would be incorporated into the pitched roofs of the two blocks. The dormers would face into the internal courtyard and therefore views of these would be limited from the public domain.

The dormers would sit centrally within the roof slope, appropriately set down from the ridge and up from the parapet and set in from the side. The material of the dormers has not been specified. The dormers proposed would show a blank façade and do not account for the composition of the windows immediately below to the elevation. The blank face to the dormer represents an unsympathetic feature. It is considered, given these internal spaces are proposed to be occupied by bathrooms, windows could be added (albeit obscurely glazed). This could be suitably controlled via condition and result in an improved overall appearance and as such would not have represented a reason for refusal. New roof lights would be modest in size and fitted close to the plane of the roof slope. Their high-level position and location within the courtyard would again screen views provided from street level.

It is therefore concluded that the proposed alteration within Application B would not have had a harmful effect on the character and appearance of the Site or the surrounding area. It would have complied with the aims of Policies 7.4 and 7.6 of the London Plan 2016, Core Policy 30 of The Enfield Plan Core Strategy 2010, Policies DMD13 and DMD37 of the Development Management Document (2014) and Policy 7.4 of the London Plan (2016) and the NPPF.

Neighbouring Amenity

London Plan policy 7.6 stated that buildings should not cause unacceptable harm to residential amenity, including in terms of privacy and overshadowing. DMD 6 and 8 required that residential developments do not prejudice the amenities enjoyed by the occupiers of neighbouring residential properties in terms of privacy, overlooking and general sense of encroachment.

CP30 of the Local Plan sought to ensure that new developments have appropriate regard to their surroundings, and that they improve the environment in terms of visual and residential amenity.

Living Conditions of Occupiers to No 73 Avenue Road

No. 73 is a terraced house sited immediately adjacent to the Site with a contemporary two storey side/rear extension that is closest to the Site boundary.

The proposed additions at roof level under Applications A and B to the flatted blocks would have extended the mass and bulk at roof level. However, the proposed alterations would have sat within the footprint of the main building and would not have extended the ridge line up beyond the existing. Whilst there would be an appreciable change to the angle of the flank elevations at roof level (particularly as a result of Application A), overall the proposals would not have resulted in loss of light, outlook or sense of enclosure, given the additionality would have sat within the height and footprint of the main buildings.

In relation to overlooking, no windows were proposed to the dormers in Application B and therefore there would be no undue overlooking to No.73. Given the orientation of the proposed dormer windows in Application A, it is considered there would be no undue overlooking to habitable windows of the adjoining property at No. 73 either. There may be some additional overlooking to the garden area, however mutual overlooking to private amenity areas is common in the area and as such would not have warranted a reason for refusal.

Living Conditions of Occupiers to Oakwood Lodge

The existing flats at ground and first floor level had habitable rooms with windows on the front and rear elevations of the buildings. The kitchens were on the 'rear' (inward facing elevation facing to the parking court). An external staircase rose to first floor level and ran close to a habitable room of a ground floor unit. The extant situation, given the external staircase, landed close to an existing rear window (within each block) and created privacy issues for the ground floor occupiers.



Image 6 - Rear of Oakwood Lodge

In Application A, the external staircase would have been altered to a spiral external staircase and increased in height to roof level. This would be the point of entrance for the proposed top floor flats and a secondary point of access for the units at first floor level. The new external staircase would be located adjacent to the doors (obscurely glazed) and side window of the existing flats at ground and first floors, which were considered to serve the kitchens. The alteration of the staircase would result in additional movements of people; however, this

would not represent a significant material intensification. Given the kitchen doors are obscurely glazed, there would be no undue overlooking. In addition, the staircase would be for access only and therefore limiting the possibility for potential overlooking. As a result, the reconfiguration of the staircase is not considered to result in unacceptable harm to the amenities of the existing occupiers of the ground and first floor flats, notably in respect of overlooking, taking in account the comparison with the extant situation.

The proposed dormer windows in Application A are designed so the habitable rooms face towards the road or the rear garden and therefore would not lead to overlooking of the flats within Oakwood Lodge. The dormer to the inward facing elevation provides for the entrance to the proposed flats only, with no habitable room windows. Whilst anyone standing on the staircase or landing platforms, would be able to look into the existing habitable rooms to the rear elevations, given their separation distance of just over 17m, this could occur with the existing staircase arrangement. The increased use of the staircase with the additional units proposed, is not considered to be significant or materially harmful to the living conditions of the existing flats.



24. Proposed Rear Elevation:

Image 7 - Application A: Rear Elevation

In Application, B access to the proposed units at roof level would be through reconfiguration of the internal access. The proposed dormers would be blank (with no windows) and therefore would not give rise to any overlooking between the Oakwood Lodge properties.

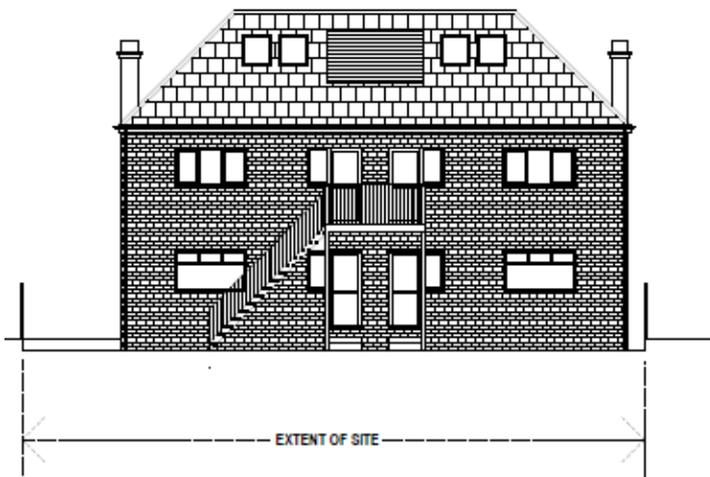


Image 8 – Application B: Rear Elevation

The cycle storage for both proposals involves the provision of an enclosed cycle store adjacent to the existing garage block, in close proximity to the rear elevation and habitable window of Block 2. The cycle store would accommodate 8 bikes and would be positioned 2m away from the rear window of the ground floor flat and would present an elevation over 4m in length. Details of the structure have not been provided.

Provision of a cycle store in this location would lead to increased activity near the habitable room window which would introduce significant privacy concerns and would not be supported. Given the overall size of the Site, it is likely that alternative provision could have been made. As such this would not form a reason for refusal and could be appropriately controlled via condition.

Notwithstanding the assessment above on the character of the area, taking into account the detailed design, form and layout of both schemes, it is considered that both Application A and B would not have had an adverse impact on the living conditions of those adjoining occupiers in terms of sunlight, outlook, sense of enclosure and privacy.

Material intensification

Comings and goings to the building would have increased as a result of the proposed developments. However, given that only a maximum of four additional units were proposed, it is considered that this would not have been to a degree that would cause significant harm to the living conditions of adjacent occupiers through additional noise or disturbance. Thus, both Applications A and B would not have caused harm to the residential character of its surroundings.

Quality of Accommodation

The schemes propose roof alterations to the two flatted blocks to facilitate the use of the attic space as 4 x1 bed units (Application A) and 2x2 bed units (Application B).

In terms of new residential development, as well as having concern for the external quality in design terms it is vital that new units are of the highest quality internally, being, amongst other things of sufficient size, functional, accessible, private, offering sufficient storage space and also be dual aspect. London Plan (2016) Policy 3.5 required that housing developments should be of the highest quality internally, externally and in relation to their context and the wider environment. Table 3.3 of the London Plan prescribed the minimum space standards for new housing.

The National Described Space Standards (2015) stated the minimum floor to ceiling height was 2.3m, for at least 75% of the Gross Internal Area (GIA). National policy on housing standards were set out in detail in the Written Ministerial Statement of 25 March 2015 (“the March 2015 WMS”). This WMS introduced a set of national technical standards, including the *Technical housing standards-nationally described space standard* (“the National Space Standard”). The March 2015 WMS stated that, “from October 2015: existing Local Plan... policies relating to...

internal space should be interpreted by reference to the nearest equivalent new national technical standard.”

Subsequent to the NDSS, The London Housing SPG expressed the minimum internal height as 2.5m for 75% of the floor area. The London Plan also encouraged a minimum standard of 2.5m for internal head heights. As of 2017, the London Plan and London Housing SPG were the most up-to-date expression of planning policy on this matter. It is a consideration to which great weight is attached in the overall planning balance.

The annotation on the submitted plans for Application A suggests a GIA for each flat of 50.8 sqm. For one bed/two person dwellings the London Plan set a minimum of 50sqm.

The plans for Application B state the overall unit sizes are 62.5sqm. The bedroom sizes are 13sqm and 11 sqm in Application B and would indicate the units are for 2b3p. The floor space quantum would exceed the minimum floor area of 61sqm for a 2b3p flat. Thus, both proposals would comply with the minimum internal space standards as set out in the London Plan.

In both applications the quantum of internal floorspace is sufficient. The London Housing SPG (2016) prescribed the minimum floor to ceiling height be 2.5m for at least 75% of the GIA. In Application A, the proposed internal ceiling height is 2.3m for 82% of the overall floor space for each unit. Whilst the applicant refers to the NDSS in support of the proposal, the London Housing SPG would have been a more recent expression of policy and therefore would have been given weight in the assessment of the proposals. Taking the above into account, the proposed units in Application A would fail to meet the London Housing SPG standard, albeit to a marginal degree; but even so the standard is clearly expressed as a minimum. Furthermore, the London Housing SPG (2016) places importance on units being dual aspect. Dual aspect dwellings with opening windows on at least two sides have inherent benefits. It is noted that the kitchen/living room would have the benefit of the dormer windows. However, the bedrooms for all the units are reliant on a skylight in the sloping roof. Whilst this may assist with providing light and ventilation, the units would fail to be dual aspect and as such this situation would be unsatisfactory and would provide the future occupiers of the flats with an unduly constrained outlook.

The maximum internal heights proposed in Application B, are 2.6m (towards the centre of the floor plan), dropping to 0.9m (close to eaves). The en-suite has a proposed internal height of 2.1m. Approximately 33sqm exceed the minimum requirement of 2.5m internal floor height, equating to 53% of the total floor area. The proposal would therefore fall below the recommended policy standard, which as stated, is expressed as a minimum. Whilst the shortfall would be small, the inadequacy in internal heights would result in a sub-standard living environment for potential future occupiers.

In addition, both units in Application B would have no windows other than rooflights set within the roof slope. These would be set approximately 1.5m from floor level. Given their exposure, it is considered these would provide high levels of natural light into the interior. It is nonetheless likely, the outlook from the flat would be

angled towards the sky. The effect combined with the undersized useable floorspace would result in the flat being an unacceptably oppressive internal environment. As a result, the proposal would offer substandard internal living accommodation for potential future occupiers.

Taking all matters into account, both developments would not achieve a satisfactory standard of residential accommodation. Accordingly, there would be conflict with London Plan Policy 3.5, CS Policy CP4, DMD policies DMD8, DMD9 and DMD37 which seek, amongst other things, to ensure occupants have sufficient internal and external space and adequate levels of light and outlook. There would also be a conflict with the NPPF which expects development to achieve a good standard of amenity and the London Housing SPG.

Private Amenity Space

Both Application A and B are reliant on the use of the communal area to the rear (535sqm) for private amenity space the proposed self-contained units. This is considered sufficient in size to cater for additional units proposed in Applications A and B and therefore would not raise any concerns in this regard.

Unit Mix

Application A proposes 4x1 bed units. The proposed mix is therefore only smaller units which fails to adequately meet the appropriate mix of units within Core Strategy Policy 5. This seeks to ensure that 'new developments offer a range of housing sizes to meet housing needs' and that the Policy should support the Council's plan for a Borough-wide mix of housing that reflects the needs and level of supply identified in the Strategic Housing Market Assessment (SHMA 2010) and subsequent reviews (2015). It considered the application is skewed towards, specifically 1-beds and therefore these considerations weigh against the merits of the scheme in the overall assessment.

Application B provides for 2x2 bed units. Whilst not strictly compliant with the housing mix as stipulated by policy, 2 bed units allow for greater flexibility and adaptability in peoples change of circumstances, over one bed units, and given the constraints of the Site, the provision of two bed weighs neutrally in the assessment of the Application B.

Transport Impact

DMD 45 relates to car parking, cycle provision and parking design. DMD 47 states that new development proposals will need to demonstrate that enough space for servicing, circulation and access to, from and through the Site is provided. All developments must be fully accessible to pedestrians and cyclists and assist with general permeability within an area. The London Plan policy 6.13, DMD policy 45 (Parking Standards and Layout) and 47 (Access, New Roads and Servicing) states that operational parking for maintenance, servicing and deliveries is required to enable a development to function.

Car Parking provision

The Site has the benefit of four garages with space in front for packed parking. The applicant has stated no additional parking is proposed to support the additional flats. The site is located within PTAL 1a/2 (low).

The maximum standards for residential parking are set out in Table 6.2 in the London Plan. For 1-2 bed properties, policy requires less than 1 parking space per unit. There were four garages on the Site with a shared, informal, courtyard space that may have accommodated further parking for the 8 existing residential units.

It is acknowledged the policy is expressed as a maximum. However, it has not been demonstrated that additional parking, in the absence of a mechanism to secure no parking, could be accommodated on Site for either proposal A or B, in conjunction with the existing car ownership and as such it is considered on street parking may occur. A Lambeth Style parking survey of the surrounding streets would have been required to demonstrate that there is room for additional parking to be accommodated on-street. The parking survey specification would therefore have needed to comply with the 'Lambeth Methodology'.

Both proposals, in this location, are not considered to demonstrate adequate information in relation to the proposed parking arrangements, nor that there would be no adverse impacts resulting from possible on-street parking. These considerations weigh negatively in the overall assessment of the applications.

Cycle provision

Table 6.3 of the London Plan sets out cycle parking provision. These comprised, 1xspace per 1xbed and 2xspaces per 2xbed+.

Both Applications result in a requirement of 8 cycle spaces. 8 spaces are indicated on both set of plans, within the courtyard area. The siting of the cycle parking, as shown would not have been appropriate due to concerns regarding overlooking, given the proximity to the ground floor unit . However, it is considered there is sufficient space on the Site for cycle parking to be relocated and this could be appropriately controlled via condition, taking into account the proposed placement of the bike storage relative to habitable windows.

Refuse

The proposed floor plans for both Applications show indicative waste and refuse storage and bins to the front of the Site adjacent to Avenue Road. Given the Site comprised flats, the proposed refuse would comprise communal waste although separated recycling and waste is not indicated on the plans.

Given the proximity to Avenue Road and the ability to store waste within the curtilage of both developments, it is considered on balance the proposed dwellings would have sufficient space and capacity for refuse and recycling.

However, given the lack of detail in these areas , in the event either proposal were considered acceptable, this would have needed to be controlled via condition and in accordance with the Local Authorities

Refuse and recycle storage in line with the Refuse and Recycle Storage Guide Enfield (ENV 08/162).

Accessible Homes

Both Applications proposed access to the units, one via external staircase and one via alteration to the internal access. London Plan requires new housing to be M4(2) compliant with the remaining 10% M4(3) complaint. Given the reliance on stepped access the proposal would not comply with the relevant standard and as result only fulfil M4(1). It is acknowledged the proposals are alterations to an existing built form and that lift access may not be feasible or viable. Nonetheless, in the assessment of the overall application, this is considered to weigh negatively.

Planning Contributions

The London Borough of Enfield no longer seeks contributions for education on schemes of 11 units and below. However, it does seek affordable housing contributions which are 10 units or less but have a combined gross floor space of more than 1000m². This is in conjunction with the criteria stipulated within the Planning Practice Guidance (PPG). The proposal in question is below the stipulated 1000m² and as such, does not require a S106 contribution towards affordable housing.

Community Infrastructure Levy

CIL would be calculated in accordance with the Mayor's adopted Community Infrastructure Levy Charging Schedule 2012 and the Enfield adopted Community Infrastructure Levy Charging Schedule 2016. The payments would be chargeable on implementation of the private housing.

Other Matters

Application A has sought to demonstrate, that the additions to the roof proposed, would be acceptable having regard to the development that was consented through the permission for the wholesale redevelopment of the adjacent site, which incorporated 3 storey houses. It is considered these are materially different schemes, given the house is different in typology to the flatted blocks and the more recent development form part of a much larger estate regeneration scheme. As such recent redevelopment which sits on a larger plot and forms part of an estate regeneration are not comparable and is not of significant weight in the assessment of the Applications A or B.

The applicant has advised that the density of development proposed in terms of habitable rooms per hectare is within an acceptable limit for Application A. However, policy states it is not appropriate to apply this mechanistically and other relevant factors include local context, design and transport capacity. Therefore, density is not a primary consideration in the assessment of the application. Taking the scheme as a whole, for the reasons outlined above, the application is considered unacceptable.

7. Conclusion

Having regard to all the above, it is considered that whilst policy seeks to increase housing units in the Borough, both developments as proposed would not have been supported.

Both Applications A and B would have resulted in poor quality accommodation for future occupiers. In addition, the alterations and extension to the roof of the blocks in Application A would have resulted in a dominant and incongruous form of development, detrimental to the character and appearance of the area.

Therefore, in overall terms, the proposals would not have met the social and environmental objectives of sustainable development, having regard to the NPPF. The proposals, would therefore, have been considered to be unacceptable on the basis below.

8. Recommendation for Application A.

Planning permission could not reasonably have been expected to be granted for the following reasons:

1. The proposed alterations to the roof form and addition of the proposed dormers, would by virtue of the design result in a dominant and discordant feature on the host buildings and in the street scene, detrimental to the character and appearance of the area. Accordingly, the development would be contrary to CP30 of the Core Strategy (2010), Policies DMD13 and DMD37 of the Development Management Document (2014) and Policy 7.4 of the London Plan (2016) and the NPPF.
2. The proposed residential units, by virtue of poor outlook and limited floor to ceiling heights would fail to meet the minimum floor space standards and result in poor-quality living accommodation. The proposal would therefore be contrary to Policy CP4 of the Core Strategy, DMD8, DMD 9 and DMD37 of the Development Management Document and Policy 3.5 including accompanying Table 3.3 of the London Plan as well as the objectives of the NPPF, the London Housing SPG.

9. Recommendation for Application B.

8.1 Planning permission could not reasonably have been expected to be granted for the following reasons:

1. The proposed residential units, by virtue of poor outlook and limited floor to ceiling heights would fail to meet the minimum floor space standards and result in poor-quality living accommodation. The proposal would therefore be contrary to Policy CP4 of the Core Strategy, DMD8, DMD 9 and DMD37 of the Development Management Document and Policy 3.5 including accompanying Table 3.3 of the London Plan as well as the objectives of the NPPF, and the London Housing SPG.

10. Alternative Appropriate Development

General Issues

10.1 Notwithstanding the above assessment, the LPA must identify any description of development which in the LPA's view would be appropriate alternative development. This could include something significantly different from what is applied for, but it is not for the LPA to carry out an exhaustive assessment of the development potential of the Site. The CAAD is intended to assist the compensation assessment by clarifying what the maximum development potential of the Site was at the RVD.

10.2 As mentioned above Section 17(5) provides that where a certificate is issued under subsection (1)(a) it must:

(a) identify every description of development that in the local planning authority's opinion would be appropriate alternative development; and

(b) give a general indication –

(i) of conditions to which planning permission for the development could reasonably have been expected to be subject.

(ii) of when permission could reasonably have been expected to be granted only at a time after the relevant valuation date, and

(iii) of any pre-condition for granting the permission (for example, entry into an obligation) that could reasonably have been expected to have to be met.

Residential Alternative Appropriate Development

10.3 The 'existing' use of the Site was residential (C3 Use Class). Policy DMD4 of the Development Management Policies seeks to resist the loss of existing residential units. There are exemptions which may be permitted subject to fulfilling certain criteria. These are:

- no net loss of residential floorspace as a result of development; or**
- to provide a community facility where there is a specific, identified need and; no alternative locations, or:**
- the continuing residential use is not satisfactory, in light of adjoining land uses and the standard of accommodation.**

10.4 Based on the policy criteria above, the loss of residential floorspace is only considered in exceptional circumstances. The London Plan and Core Strategy make a commitment to deliver new housing and prevent the loss of existing units. Existing residential land and buildings play an important role in meeting the borough's housing needs, particularly the needs of families.

10.5 Policy identifies community uses as a possible exceptional circumstance to the loss of residential, being noted as a possible alternative acceptable use, subject to certain criteria. In this instance the applicant would have had to demonstrate that a specific need had been identified and no alternative locations (in the borough) would be available for the loss of residential to be considered and permitted.

10.6 It is considered highly unlikely the policy would be satisfied, to permit the loss of residential units in this location. The loss of the residential units could have only been supported by a suitable community facility of specific need identified, which demonstrated, with supporting

evidence there were no alternative sites anywhere else in the borough. Furthermore, the surrounding area to the Site is largely residential and therefore the Site itself is conducive to continued residential use. There is no information to suggest the Site was not suitable for continued residential use, in light of adjoining land uses (such as industrial) or the units were of poor standard of accommodation. As such, it is considered the loss of suitable self-contained accommodation would have been strongly resisted given the weight of the policy for retention of existing residential units in Planning Policy and as such, there are no realistic appropriate alternative uses for the Site, other than residential (C3 Use Class).

10.7 In the Officers opinion, and on the basis of the information submitted within the application for the CAAD it is considered that the following Use Class C3 (Dwellinghouses) would have been appropriate alternative development for the Site:

(a) 2x1 bed units (C3 use class) and erection of dormer windows.

10.8 Article 3(3) of the Land Compensation Development (England) Order 2012 requires an LPA to give reasons for any decision to issue a CAAD for development otherwise than as described in the application or contrary to representations in writing.

10.9 It is therefore the Officer's view that based on the plans submitted with both applications; that the Site is of a sufficient size to accommodate a 1 bed flat in each block in connection with dormer windows (to increase usable floor area). The floors to Application B indicate a floor area of 33sqm. Relative to a 50sqm flat the proposal would equate to 66% of floor area above 2.5m. It is considered with the addition of dormers, these could be redesigned and reconfigured to be acceptable in design terms and allow for the units to be dual aspect. In relation to additional car parking requirements it is considered the likelihood of two cars may be accommodated on Site. Moreover, if there was overspill of parking the likely minor impacts could be accommodated via on street parking.

11. Indication of Conditions

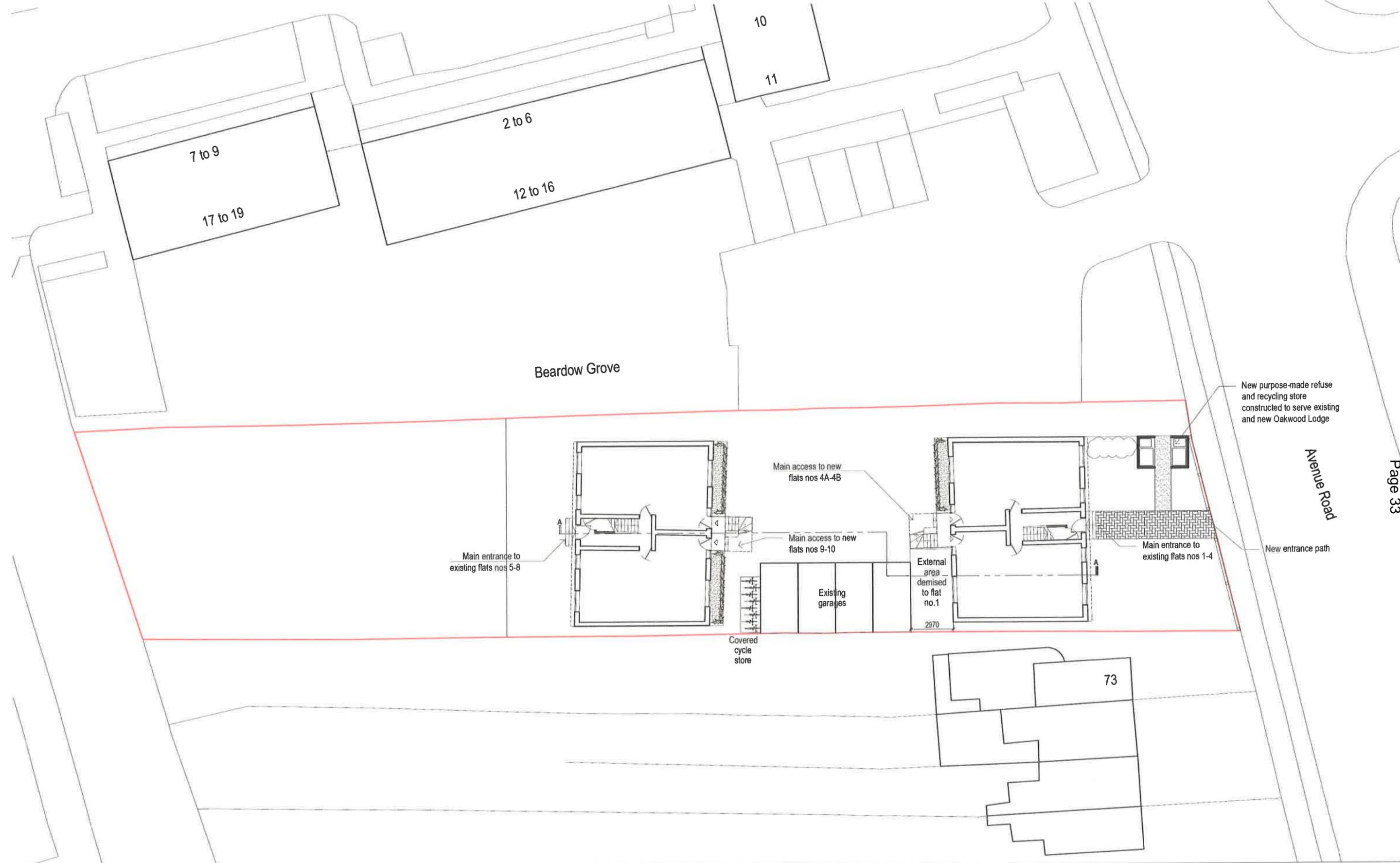
11.1 Guidance suggests that if giving a positive certificate, the local planning authority must give a general indication of the conditions and obligations to which planning permission would have been subject. As such the general indication of conditions and obligations to which the planning permission could reasonably be expected to be granted should focus on those matters which affect the value of the Site. Conditions relating to detailed matters such approval of external materials or landscaping would not normally need to be indicated. However, clear indications should be given for matters which do affect the value of the land, wherever the authority is able to do so. The conditions attached below would not affect the value of the land.

1. Time Limited Permission
2. Development to be carried out in accordance with approved plans
3. Materials and samples
4. Details of refuse
5. Details of cycle storage

12. Conclusion

12.1 That a positive Certificate be issued under section 17(1)(a) of Part III of the Land Compensation Act 1961, as amended by Part 9 of the Localism Act 2011, indicating that in the Local Planning Authority's opinion there is development, for the purposes of section 14 of the Land Compensation Act 1961 that is appropriate in relation to the acquisition and that planning permission would have been granted, subject to the conditions detailed below in this report, for development comprising of:

- **2x 1bed self-contained units (1 unit within each roof space) and erection of dormer windows and skylights.**



General Notes:

1. Please do not scale from this drawing.
2. All dimensions to be checked on site by the contractor.
3. All references to drawings refer to current revision of that drawing.
4. Structural services information shown is indicative only. Refer to consultants drawings for details and setting out.
5. All work and materials to be in accordance with current applicable statutory legislation and to comply with all relevant codes of practice and British standards.
6. Please report all drawing errors and omissions to the architect.

Rev Notes:

A. XX.XX.XX - XXXX



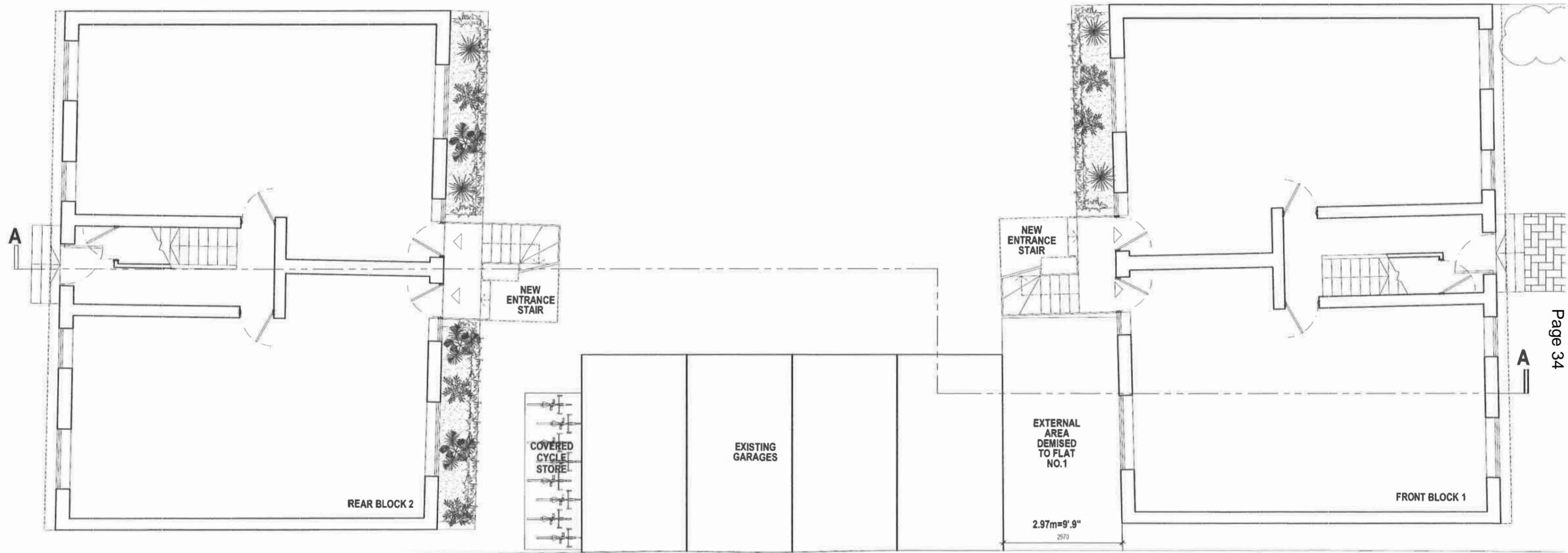
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Project:
OAKWOOD LODGE DEVELOPMENT, AVENUE ROAD, N14 4DE
 Drawing Title:
PROPOSED ROOF CONVERSION - SITE PLAN

Date:
 18.07.18

Scale:
 1:250@ A3

Drawing No:
517-100-P1



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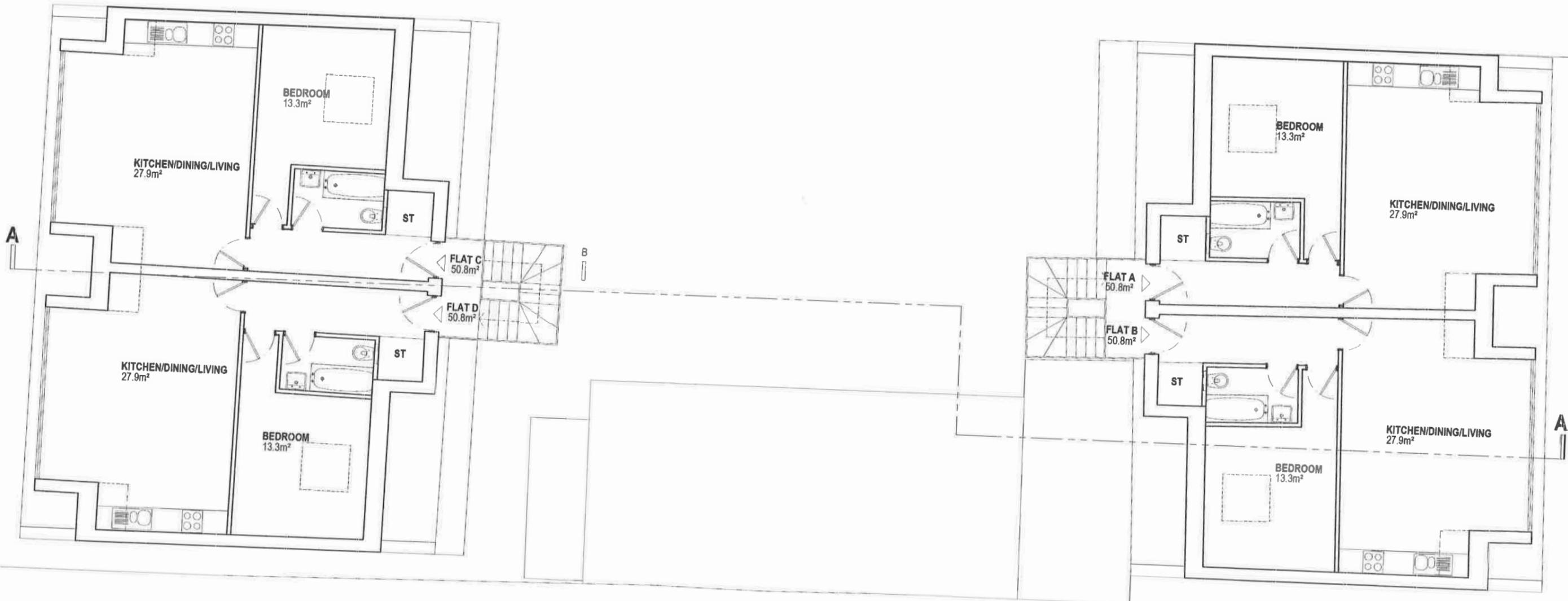
A. XX.XX.XX - XXXX



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Project:
OAKWOOD LODGE DEVELOPMENT, AVENUE ROAD, N14 4DE
 Drawing Title:
PROPOSED ROOF CONVERSION - GROUND FLOOR PLAN
 Date: 18.07.18 | Scale: 1:100@ A3

Drawing No:
517-101-P1



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Rev Notes:

A. XX.XX.XX - XXXX



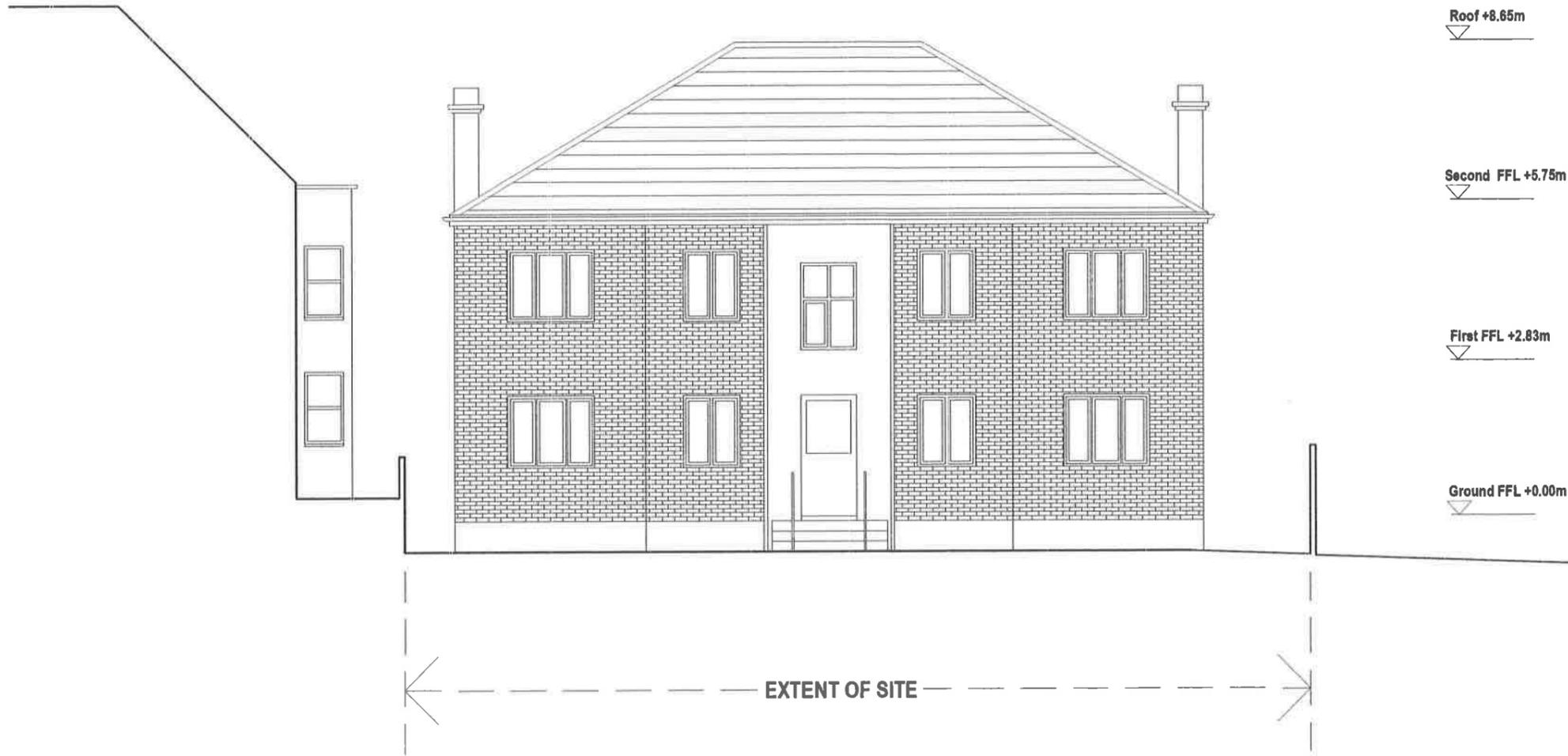
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Project: **OAKWOOD LODGE DEVELOPMENT, AVENUE ROAD, N14 4DE**
 Drawing Title: **PROPOSED ROOF CONVERSION - SECOND FLOOR PLAN**

Date: 18.07.18 Scale: 1:100@A3

Drawing No: **517-102-P1**



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6. Please report all drawing errors and omissions to the architect.

Rev Notes:

A. XX.XX.XX - XXXX



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Project:
OAKWOOD LODGE DEVELOPMENT, AVENUE ROAD, N14 4DE
 Drawing Title:
PROPOSED ROOF CONVERSION: EXISTING FRONT ELEVATION
 Date: 18.07.18 | Scale: 1:100 @ A3 | Drawing No: **517-105-P1**



General Notes:

1. Please do not scale from this drawing.
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3. All references to drawings refer to current revision of that drawing.
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Rev Notes:

A. XX.XX.XX - XXXX



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Project:
OAKWOOD LODGE DEVELOPMENT, AVENUE ROAD, N14 4DE
 Drawing Title:
PROPOSED ROOF CONVERSION - PROPOSED FRONT ELEVATION

Date:
 18.07.18 | Scale:
 1:100 @ A3

Drawing No:
517-108-P1



- General Notes:**
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Rev Notes:
A. XX.XX.XX - XXXX

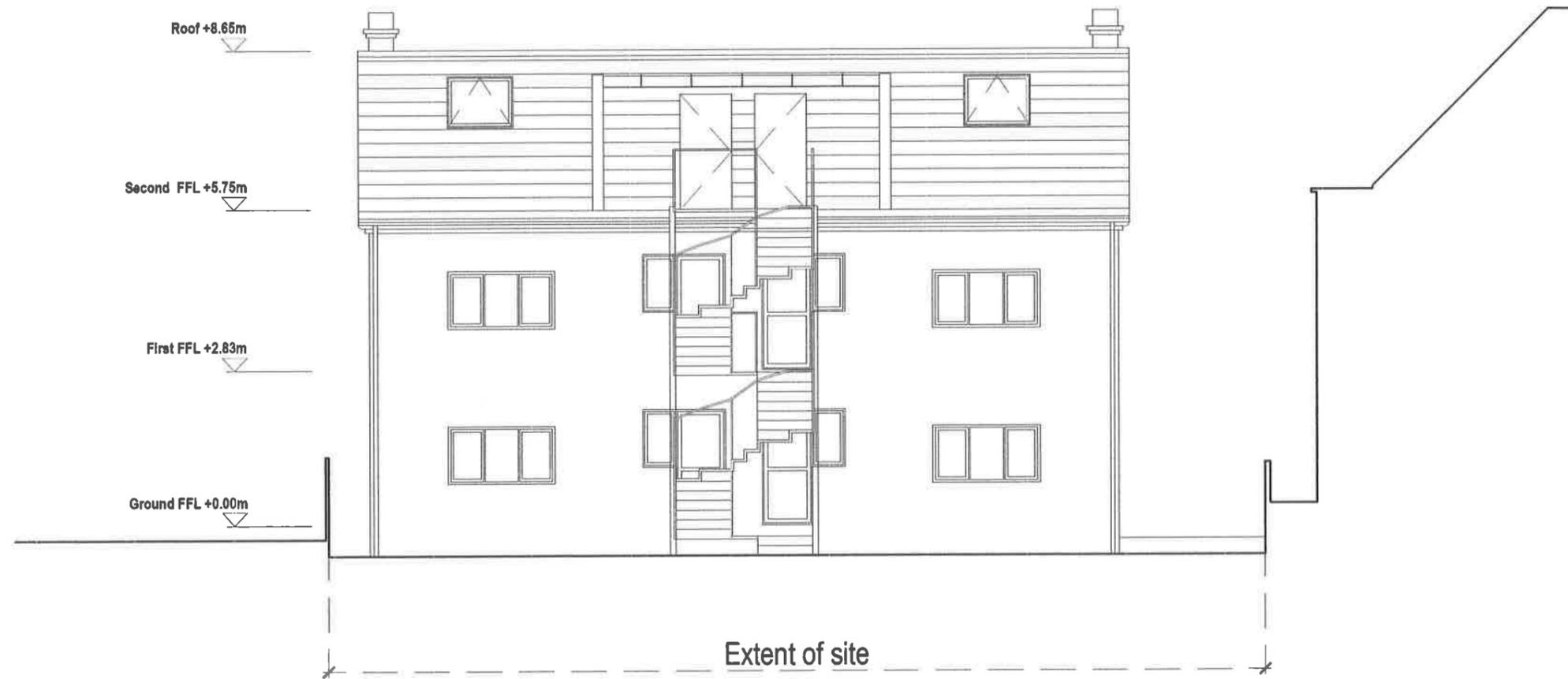


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Project:
OAKWOOD LODGE DEVELOPMENT, AVENUE ROAD, N14 4DE

Drawing Title:
PROPOSED ROOF CONVERSION: EXISTING REAR ELEVATION

Date: 18.07.18 | Scale: 1:100 @ A3 | Drawing No: **517-106-P1**



General Notes:

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Rev Notes:

A. XXXXX - XXXX



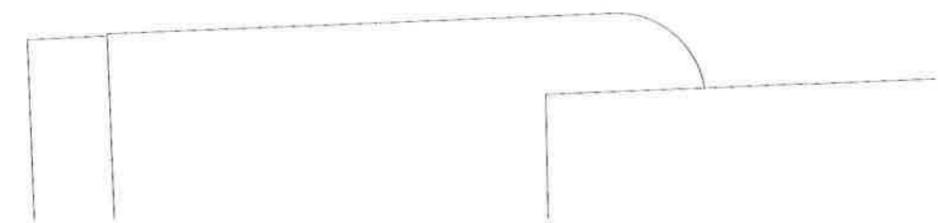
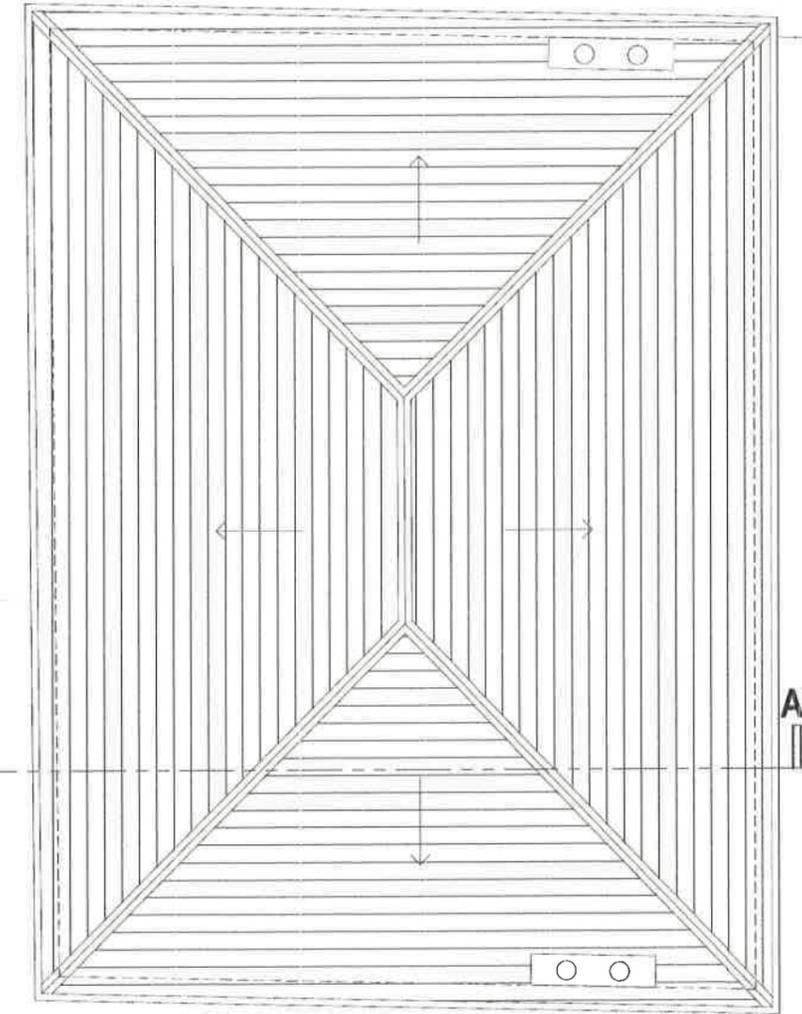
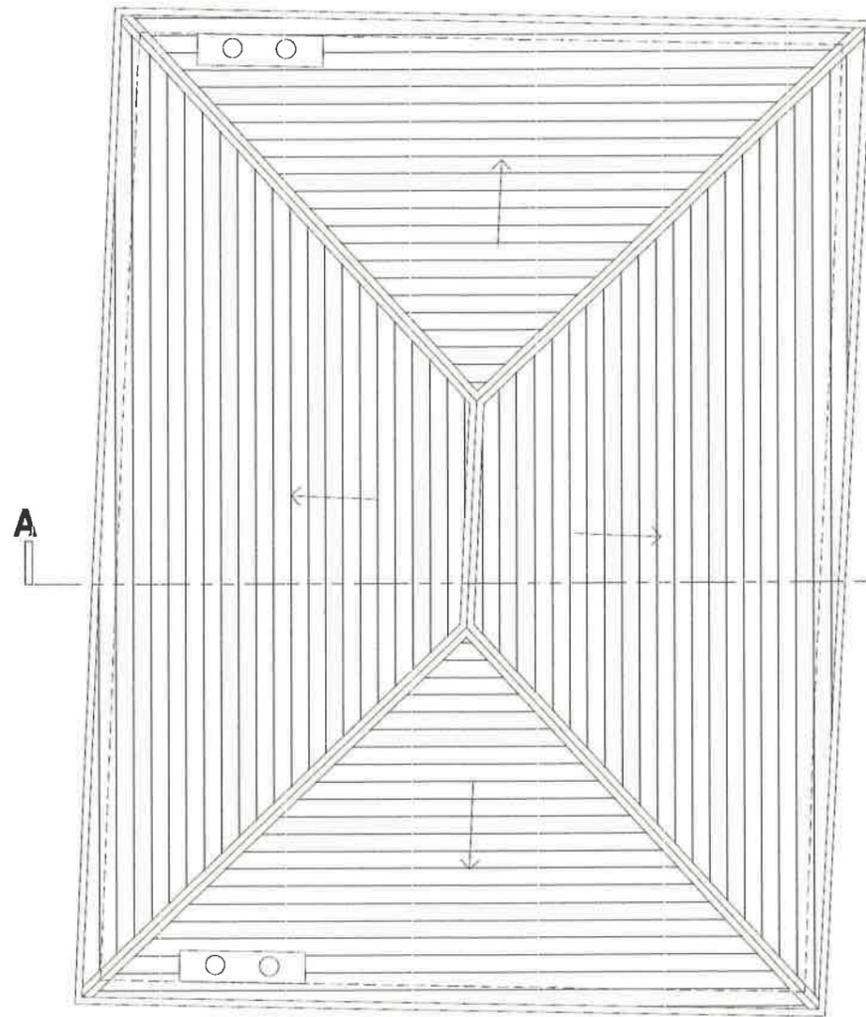
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Project:
OAKWOOD LODGE DEVELOPMENT, AVENUE ROAD, N14 4DE
 Drawing Title:
PROPOSED ROOF CONVERSION - PROPOSED REAR ELEVATION

Date:
 18.07.18

Scale:
 1:100 @ A3

Drawing No:
517-109-P1



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Rev Notes:

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Project: OAKWOOD LODGE DEVELOPMENT, AVENUE ROAD, N14 4DE

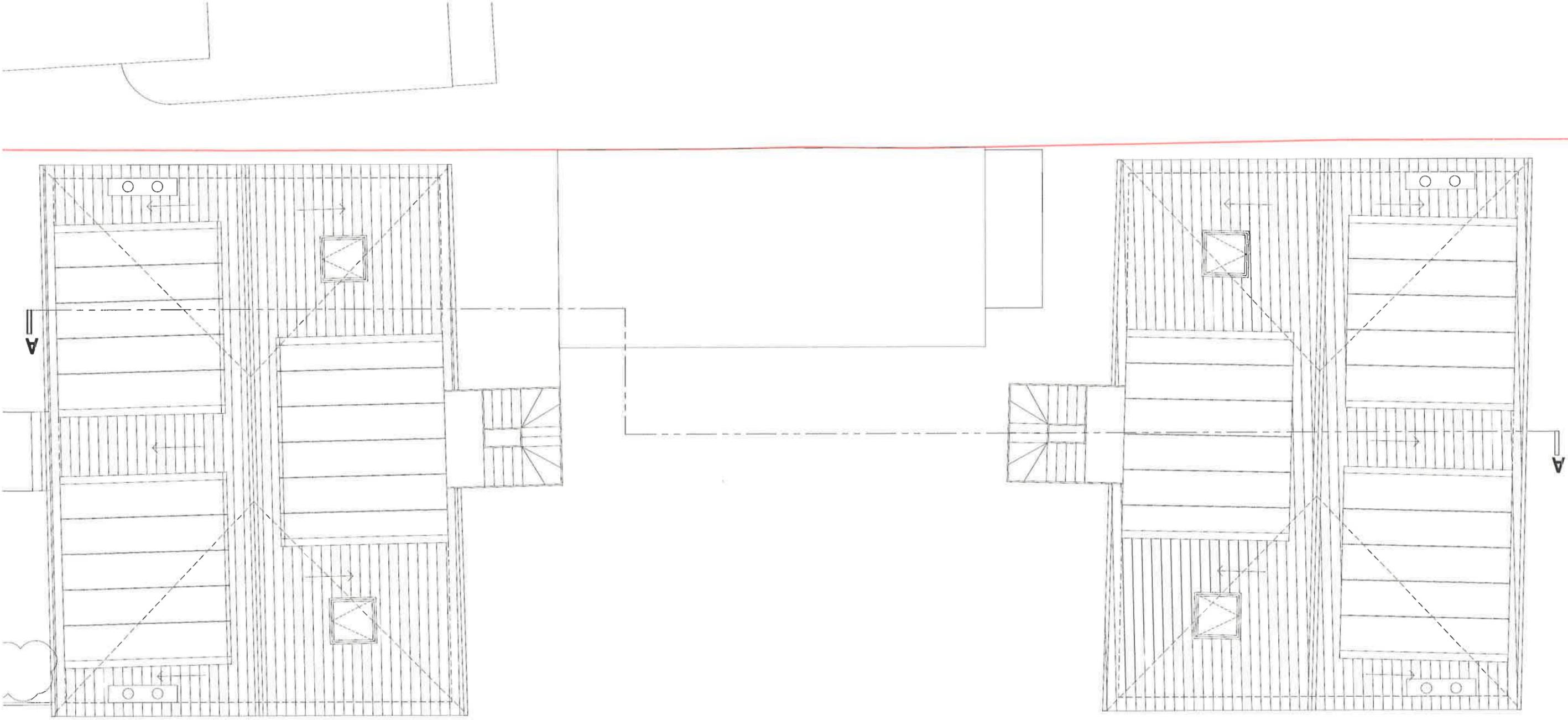
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Date: 10.07.18 Scale: 1:100@ A3

Drawing No: 517-103-P1

- General Notes:**
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Rev Notes:
A. XXXXXX - XXXX



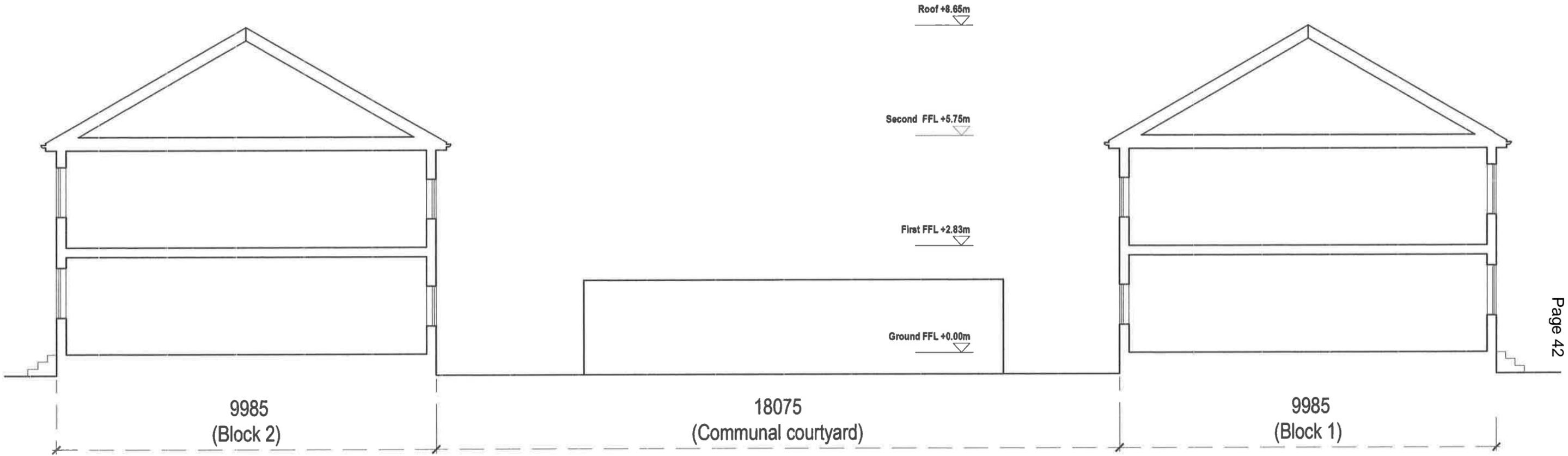
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Project:
 OAKWOOD LODGE DEVELOPMENT, AVENUE ROAD, N14 4DE

Drawing Title:
 PROPOSED ROOF CONVERSION - PROPOSED ROOF PLAN

Drawing No.: 517-104-P1

Date: 10.07.18
Scale: 1:100 @ A3



General Notes:

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Rev Notes:

A. XX.XX.XX - XXXX



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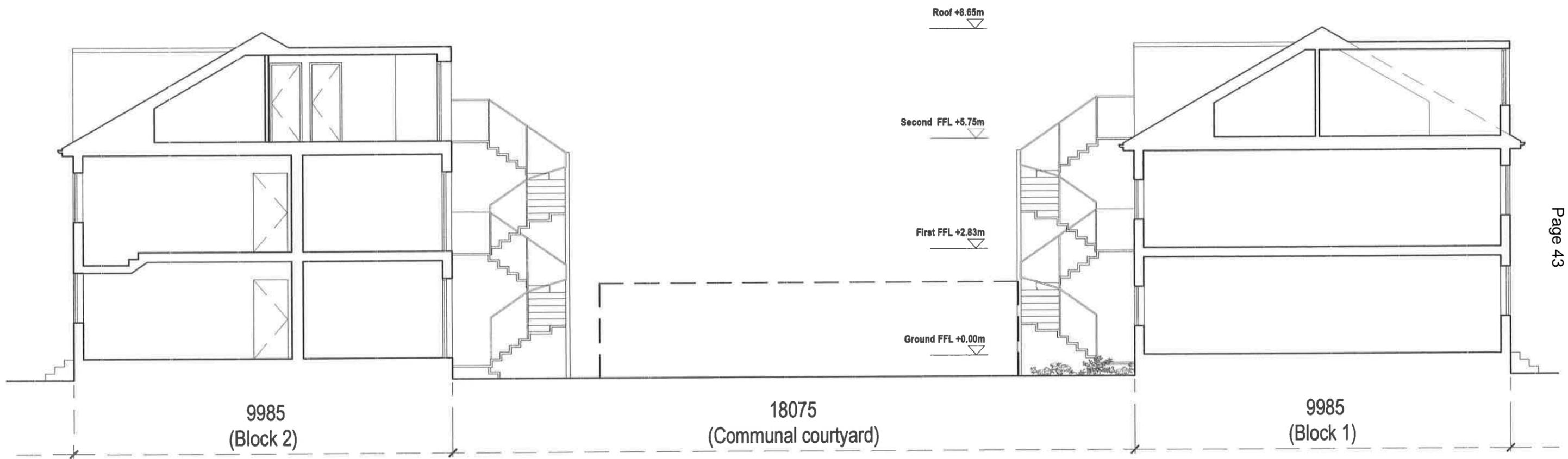
Project: OAKWOOD LODGE DEVELOPMENT, AVENUE ROAD, N14 4DE

Drawing Title: PROPOSED ROOF CONVERSION: EXISTING SECTION AA

Date: 18.07.18

Scale: 1:100 @ A3

Drawing No: 517-107-P1



General Notes:

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Rev Notes:

A. XXXXX - XXXX



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Project: **OAKWOOD LODGE DEVELOPMENT, AVENUE ROAD, N14 4DE**

Drawing Title: **PROPOSED ROOF CONVERSION - PROPOSED SECTION AA**

Date: 18.07.18

Scale: 1:100 @ A3

Drawing No: **517-110-P1**



Key:

 Application Site



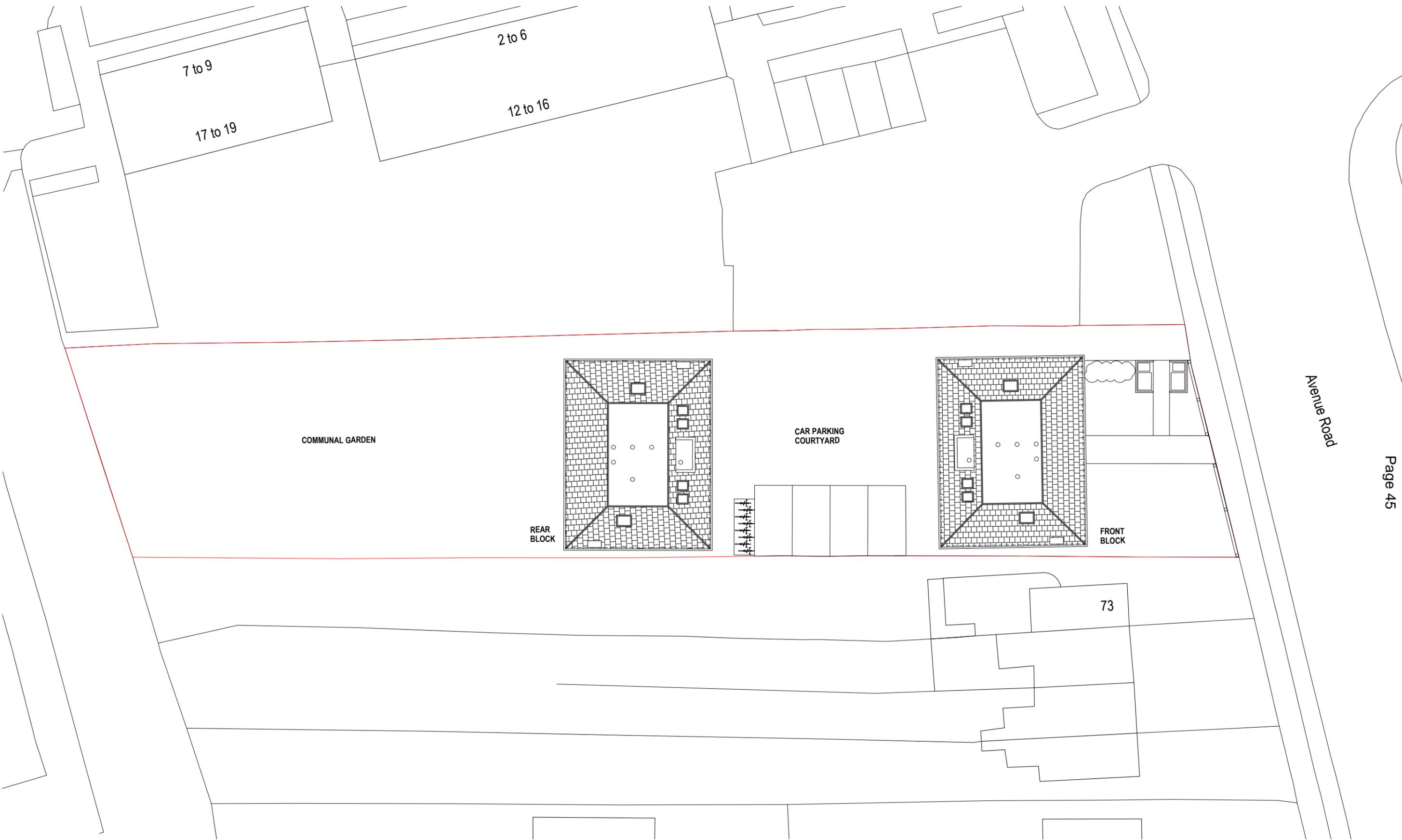
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Project:
OAKWOOD LODGE, AVENUE ROAD, LONDON N14 4DE

Drawing Title:
PROPOSED ROOF CONVERSION - SITE PLAN

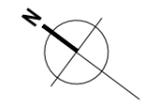
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Drawing No:
568-LOC-P1



- General Notes:
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PRELIMINARY

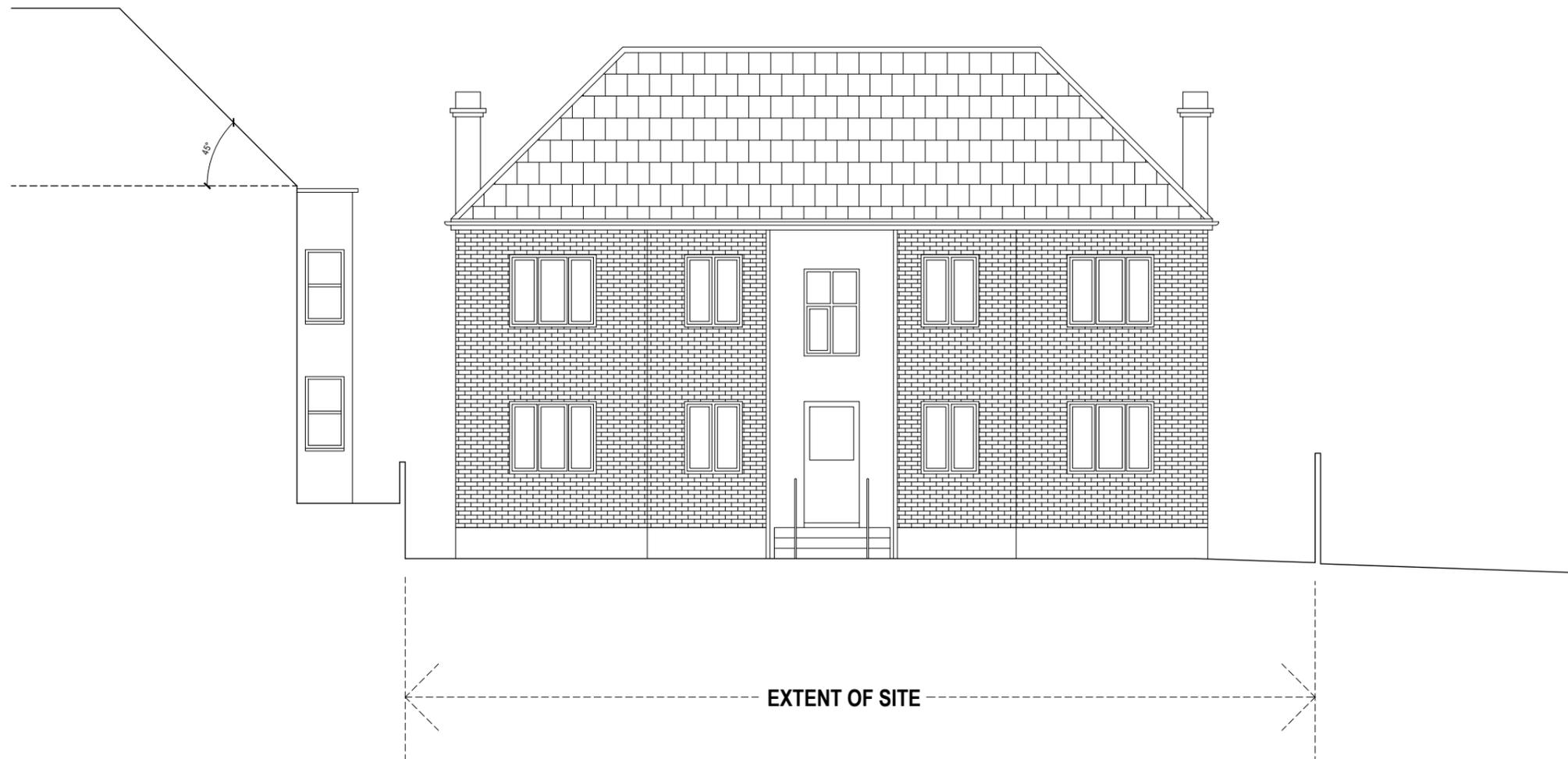


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Project:
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Drawing Title:
PROPOSED ROOF CONVERSION - SITE PLAN

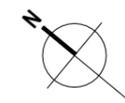
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General Notes:

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PRELIMINARY



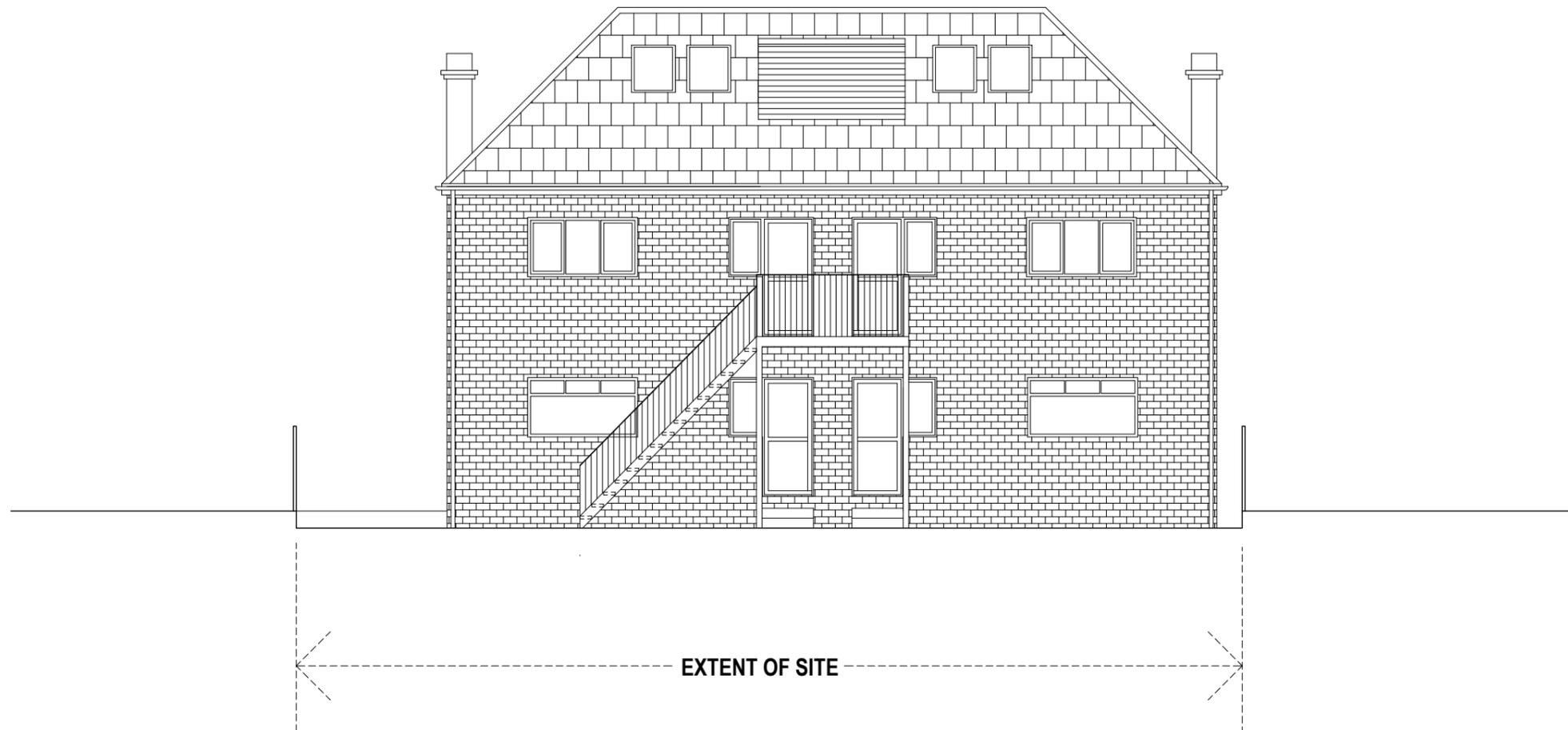
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Project:
OAKWOOD LODGE, AVENUE ROAD, LONDON N14 4DE

Drawing Title:
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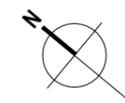
Date: 16.09.20 | Scale: 1:100 @ A3

Drawing No:
568-102-P1



- General Notes:
1. Please do not scale from this drawing.
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PRELIMINARY



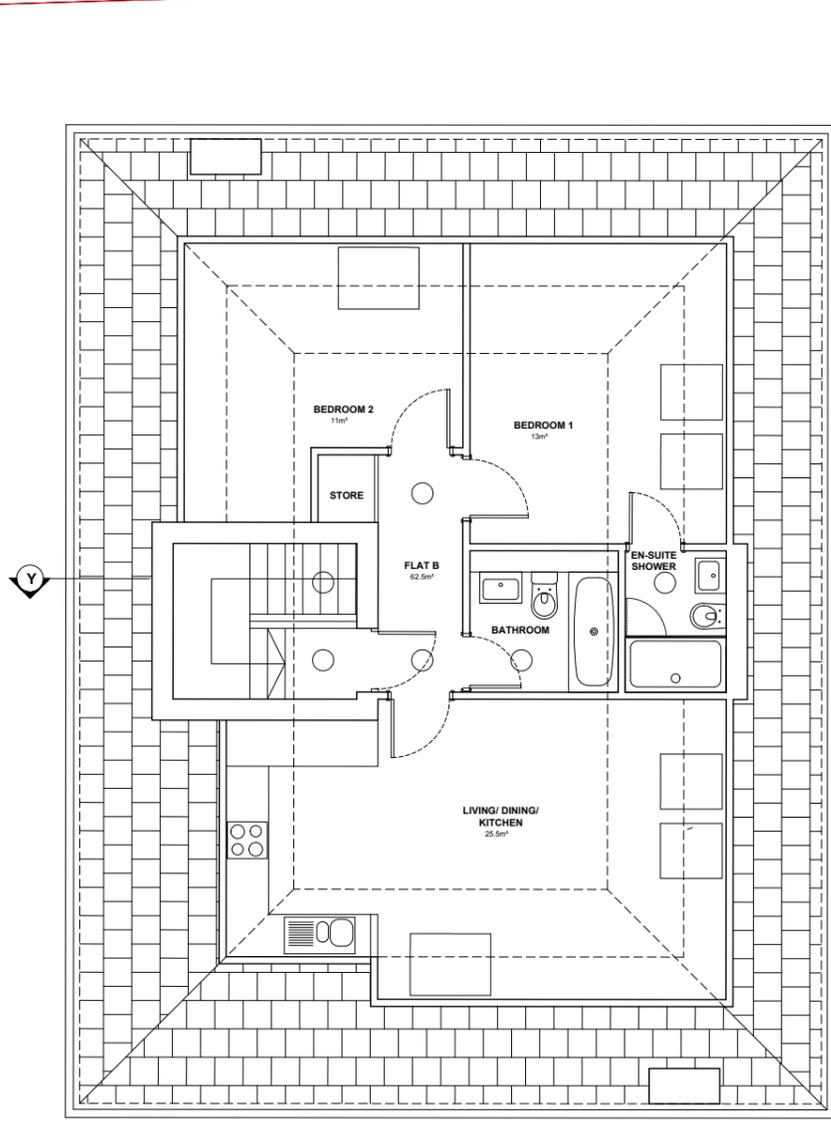
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Project:
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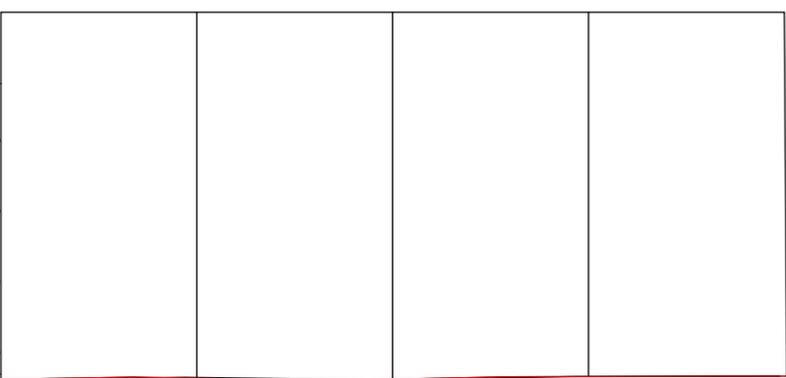
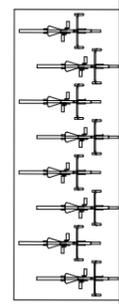
Drawing Title:
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Date:
16.09.20 Scale:
1:100 @ A3

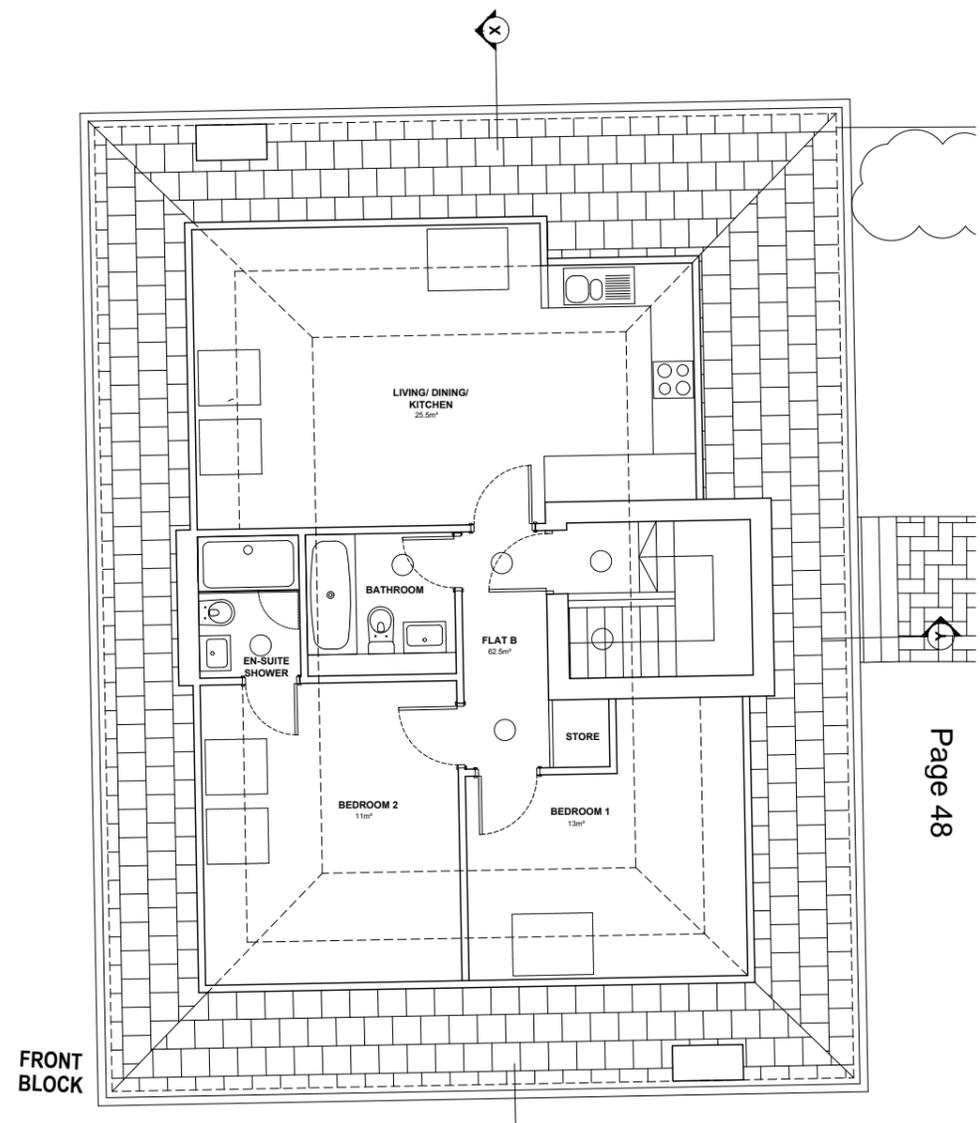
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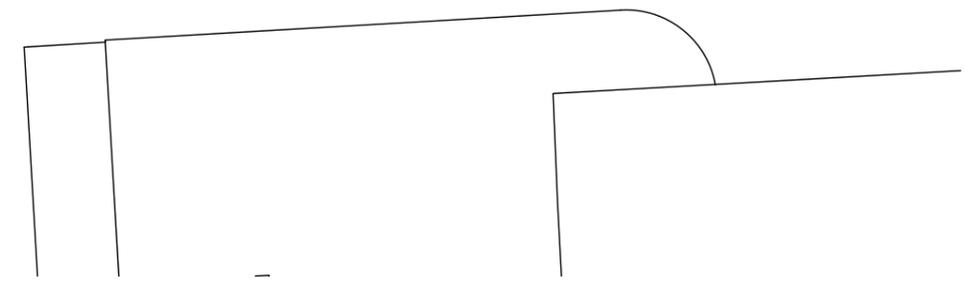
REAR
BLOCK



CAR PARKING
COURTYARD



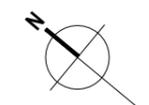
FRONT
BLOCK



Page 48

- General Notes:
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PRELIMINARY

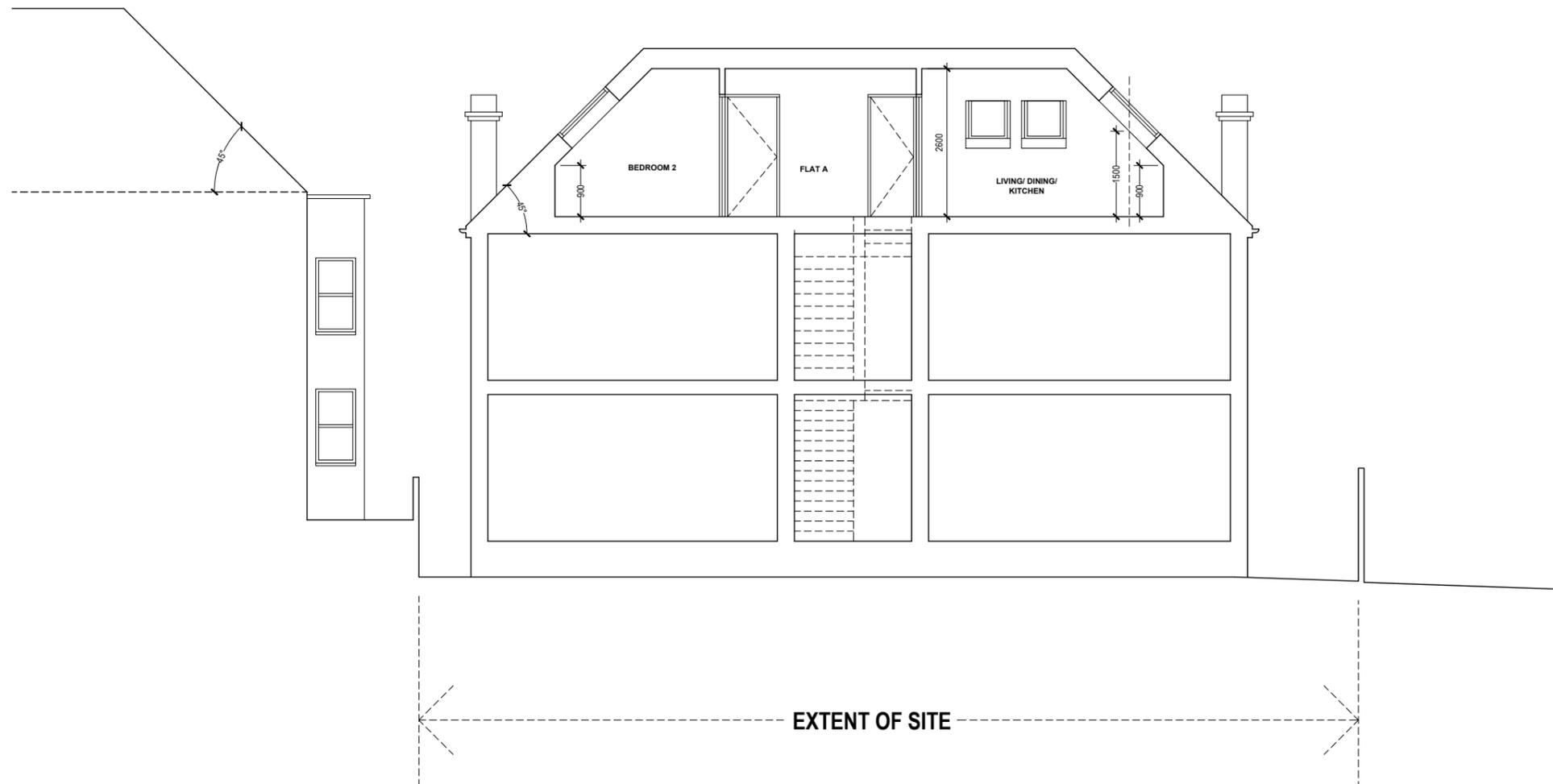


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Project:
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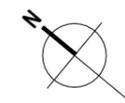
Drawing Title:
PROPOSED ROOF CONVERSION - SECOND FLOOR PLAN

Date: 16.09.20 | Scale: 1:100 @ A3 | Drawing No: 568-101-P1



- General Notes:
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PRELIMINARY

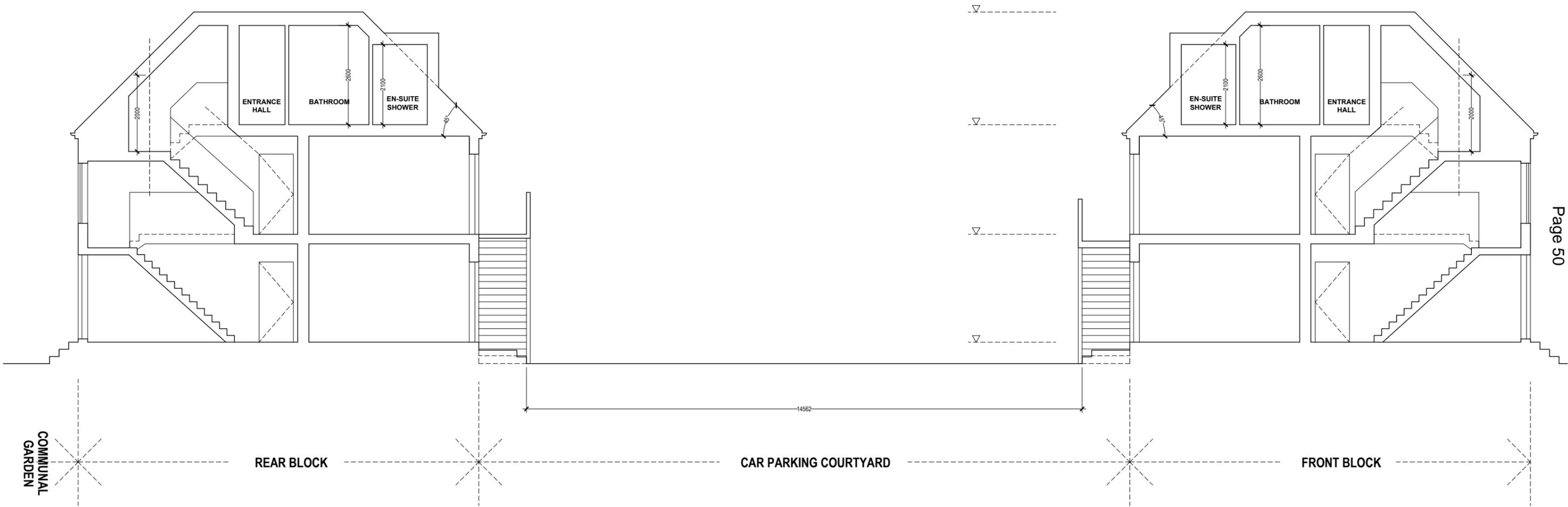


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Project:
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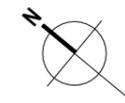
Drawing Title:
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Date: 16.09.20 | Scale: 1:100 @ A3 | Drawing No: 568-103-P1



- General Notes:
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PRELIMINARY



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Drawing Title:
PROPOSED ROOF CONVERSION - SECTION Y-Y

Date: 16.09.20 | Scale: 1:100 @ A3 | Drawing No: 568-105-P1



Key:

 Application Site



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Drawing Title:
PROPOSED ROOF CONVERSION - SITE PLAN

Date: 16.09.20 Scale: 1:1250 @ A4

Drawing No:
568-LOC-P1

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LONDON BOROUGH OF ENFIELD

PLANNING COMMITTEE

Date: 2 February 2021

Report of
Head Of Planning

Contact Officer:
Andy Higham
Sharon Davidson
Tel: 0208 132 2714

Ward: Ponders
End

Application Number: 20/03070/FUL

Category: Minor

LOCATION: FALCON ROAD SPUR, EN3 4LX

PROPOSAL: Reconfiguration of existing car park layout to provide car park spaces for residential, medical centre and community uses to be provided by Phase 2 of Alma Estate development, formation of vehicular access; and drop-off facility.

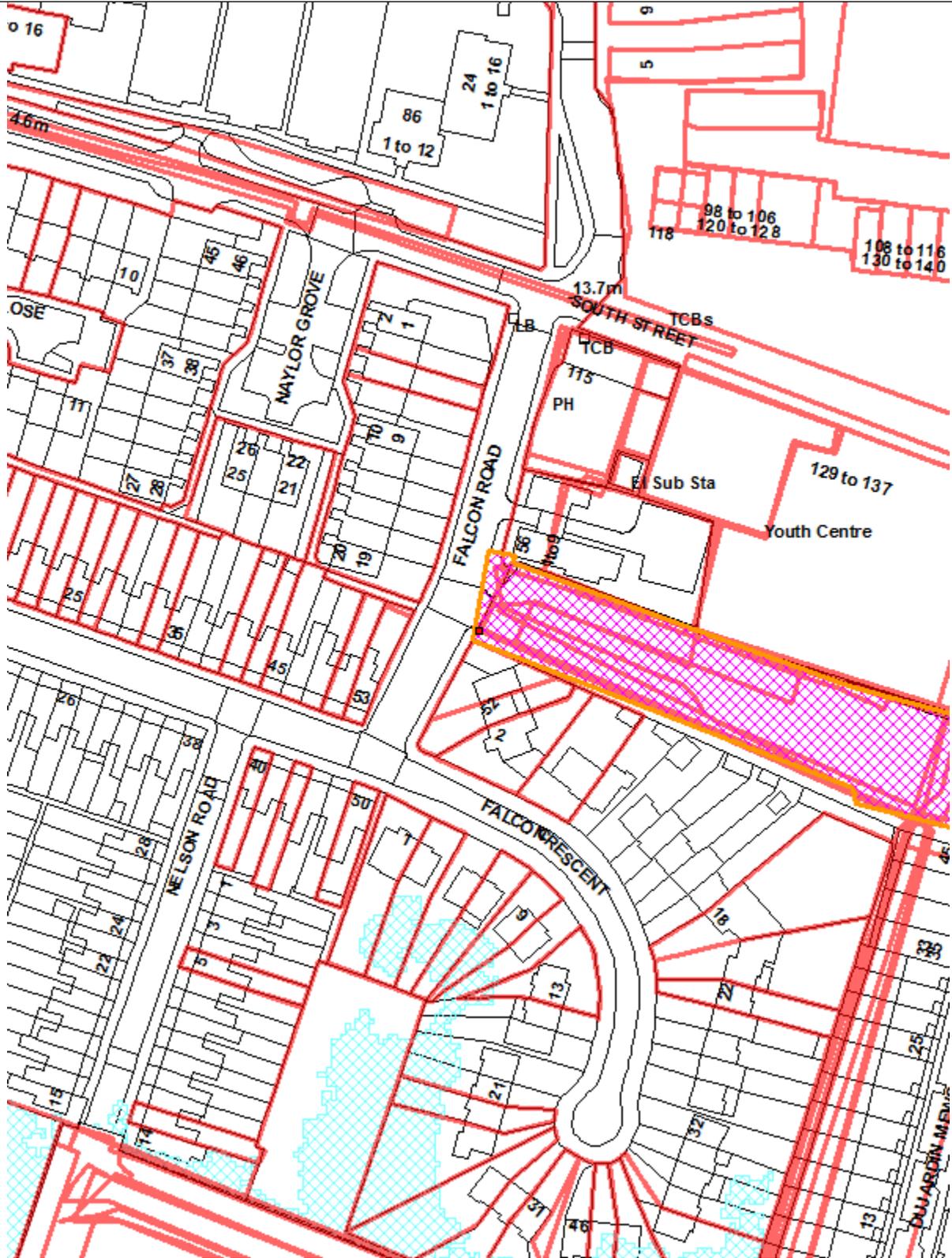
Applicant Name & Address:
Mr Alex Cook,
Countryside Properties (UK) LTD.
C/O Agent

Agent Name & Address:
Mr Greg Blaquiere,
Terence O'Rourke,
7 Heddon Street,
London,
BH7 7DU

RECOMMENDATION:

That subject to the completion of a Deed of Variation to the Section 106 Agreement, the Head of Development Management / Planning Decisions Manager be authorised to GRANT planning permission subject to conditions

Ref: 20/03070/FUL LOCATION: Falcon Road Spur, Enfield , EN3 4LX,

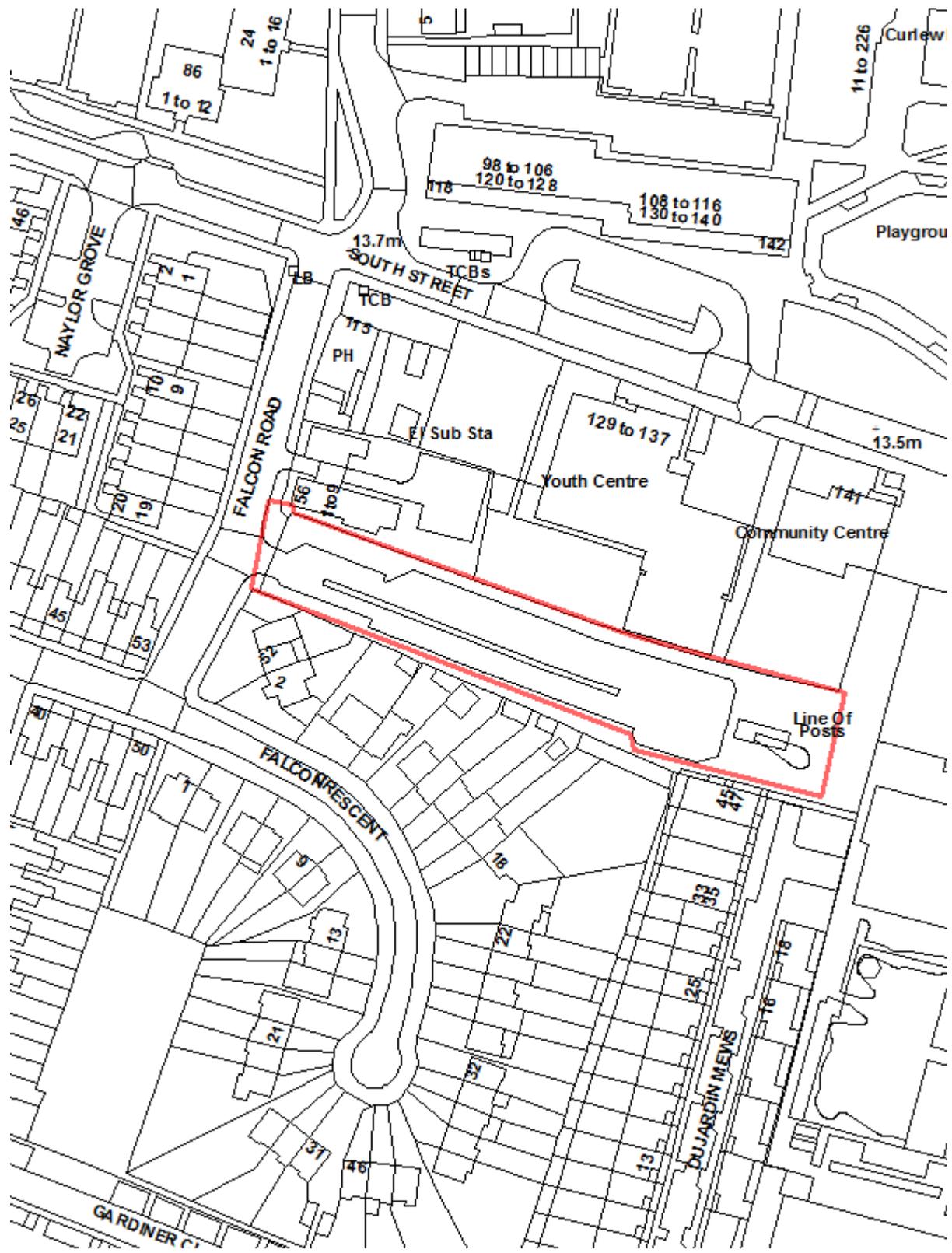


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Scale 1:1250

North





1 Note for Members

- 1.1 This application was reported to Planning Committee on 15th December 2020 when Members deferred consideration until consultation had been undertaken with Ward Councillors in light of concerns regarding the overall distribution and availability of car parking spaces across the range of uses and the potential impact on the operation of the youth and community centre. Consultation has now taken place with Ward Councillors and meetings have been held between Ward Councillors and the Regeneration Team to clarify the need for and shared use of the additional spaces.
- 1.2 The revised planning application seeks to provide sufficient car parking spaces to enable the medical centre to deliver high quality care to patients on site and at their home via on call visits, alongside enough spaces for whoever manages the youth and community centres to staff and operate effectively.
- 1.3 The application is being brought back to Members for decision. The planning application is reported to Planning Committee as the Council has an interest in the land

2 Executive Summary

- 2.1 The car park layout contained in this application was approved in August 2018 under reference 17/04816/FUL. This application formed part of a group of applications considered in relation to the Reserved Matters for Phases 2A, 2Ai and 2Aii and the S73 Variation to the original outline application for the Alma Estate Regeneration Project.
- 2.2 This application proposes an amendment to that application (ref 17/0816/FUL) to enable the allocation of 10 car parking spaces for the use of staff in the medical centre to be provided in Woodall Road, within Phase 2Ai, already approved as part of the Alma Estate Regeneration Project. The total number of car parking spaces and the layout will remain the same but the introduction of 10 spaces for the medical centre will result in a reduction of 5 residential car parking spaces and 5 community spaces. The drop off and pick up spaces contained in the turning circle for the Oasis Academy will remain unchanged.
- 2.3 The reasons for recommending approval are:
 1. The reallocation of the car parking spaces to the medical centre use does not result in any unacceptable adverse impact to the safety and capacity of the surrounding highways network
 2. The development would not result in any unacceptable adverse harm to the residential amenity of surrounding residents in terms of noise, loss of privacy or disturbance.

3 Recommendation

- 3.1 That subject to the completion of a Deed of Variation to the S106 to secure the matters covered in this report, the Head of Development Management/ the Planning Decisions Manager be authorised to GRANT planning permission subject to the following conditions.

1. The development hereby permitted shall be carried out in accordance with the approved plans including plans(s) that may have been revised or may be amended as set out in the attached schedule which forms part of this notice.
Reason: For the avoidance of doubt and in the interests of proper planning.
2. No development shall take place until plans detailing the existing and proposed ground levels including the levels of any proposed buildings, road and or hard surfaced area shall be submitted to and approved in writing by the Local Planning Authority. The development shall be constructed in accordance with the approved details.
Reason: To ensure that levels have regard to the level of surrounding development, gradients and surface water drainage.
3. Within 3 months of commencement of works, but prior to any occupation of any part of the site details of any external lighting proposed shall be submitted to and approved in writing by the Local Planning Authority. The approved external lighting shall be provided before the development of that part of the site is occupied.
Reason: To ensure that the development does not prejudice the amenities of adjoining occupiers and / or the visual amenities of the surrounding area.
4. No development shall take place until full details of both hard and soft landscape proposals have been submitted to and approved by the Local Planning Authority. The landscape details shall include:
 - Details of measures to prevent over-running by vehicles of landscaped areas.
 - Protective measures to prevent over-running of vehicles of pedestrian areas.
 - Planting plans.
 - Written specifications (including cultivation and other operations associated with plant and grass establishment)
 - Schedules of plants and trees, to include native and wildlife friendly species and large canopy trees in appropriate locations (noting species, planting sizes and proposed numbers / densities)
 - Full details of tree pits including depths, substrates and irrigation systems (N.B. We intend to issue the Enfield Tree Officer's preferred design for tree pits with the Decision Notice)
 - The location of underground services in relation to new planting
 - A maintenance and management strategyAll hard and soft landscape works shall be carried out in accordance with the approved details and to a reasonable standard in accordance with the relevant recommendations of appropriate British Standards or other recognised Codes of Good Practice. The works shall be carried out prior to the occupation of any part of the development on the relevant part of the site or in accordance with the timetable agreed with the Local Planning Authority. Any trees or plants that, within a period of five years after planting, are removed, die or become, in the opinion of the Local Planning Authority, seriously damaged or defective, shall be replaced as soon as is reasonably practicable with others of species, size and number as originally approved, unless the Local Planning Authority gives its written consent to any variation.
Reason: To ensure the provision of amenity, and biodiversity enhancements, to afforded by appropriate landscape design, and to increase resilience to the adverse impacts of climate change the in

line with Core Strategy policies CP36 and Policies 5.1 - 5.3 in the London Plan.

5. The development shall not commence details of surface drainage works have been submitted and approved in writing by the Local Planning Authority. The details shall be based on an assessment of the potential for disposing of surface water by means of a sustainable drainage system in accordance with the principles as set out in the Technical Guidance to the National Planning Policy Framework and shall be designed to a 1 in 1 and 1 in 100 year storm event allowing for climate change. The drainage system shall be installed/operational prior to the first occupation of that part of the development and a continuing management and maintenance plan put in place to ensure its continued function over the lifetime of the development. The development shall be carried out strictly in accordance with the details so approved and maintained as such thereafter.
Reason: To ensure the sustainable management of water, minimise flood risk and to minimise discharge of surface water in accordance with Policy CP28 of the Core Strategy, DMD61 of the Development Management Document, Policies 5.12 & 5.13 of the London Plan and the NPPF.

- 6 The development shall not commence (including the erection of site hoardings restricting access to Falcon Road Spur) until a Construction Logistics Plan has been submitted to and approved in writing by the local planning authority . This CLP to include measures to deal with temporary arrangements to allow access to a drop off area for the school
The plan should also include:
 - a. Background and key issues - Details of the applicant, name of the site, overview of the site and key issues to be addressed.
 - b. Site information - This should include the location of the site, and the size and nature of the development. A photographic condition survey of public carriageways, verges and footways in the vicinity of the site is also required.
 - c. Works programme - Details of the scheme including indicative dates for each stage of the construction process.
 - d. Possible trip generation - The developer will need to identify the number of trips associated with the construction project.
 - e. Routeing - Details of primary and secondary designated routes to show how vehicles will keep to main routes and comply with the restrictions of the London Lorry Control Scheme.
Monitoring the use of these routes is also required.
 - f. Delivery scheduling - The developer will need to efficiently manage the transport of supplies to the construction site;
 - g. Use of holding areas and vehicle call off - Where necessary developers should make provision for vehicles to be held off-site, acknowledging and taking into account local and route restrictions, and ensuring there is no on-road queuing.
 - h. Permit schemes and access - If required to manage the site effectively.
 - i. Impact on the highway - If changes to the highway are necessary for construction access. This should include maps of the extent of any encroachment on the public highway and related changes to routeing and signage.
 - j. Swept Path Analysis - Details of a swept path analysis for construction vehicles.
 - k. Parking, loading and unloading arrangements - It is necessary to include details of any parking bay suspensions needed to allow construction vehicles to enter and leave the site.

Also refer to any specific parking, loading and unloading arrangements.

l. Hours of operation - The CLP should provide details of the hours of operation that

construction activities will be limited to.

m. Traffic management - Information on how traffic will be managed during the various phases of the construction, including the type of construction vehicles to be used and when, parking arrangements for delivery vehicles, pedestrian cyclists, bus and general traffic considerations.

n. Measures and training to reduce the danger posed to cyclists by HGVs - This should include membership of the Freight Operators Recognition Scheme or an approved equivalent.

o. Dust and dirt minimisation - Details of actions being taken to minimise the transfer of dust and dirt including construction vehicle wheel cleaning methodology and facilities.

p. Waste minimisation - Actions to be taken to minimise the creation and transfer of waste from the site.

q. Use of alternative modes of transport - show consideration of using water freight and rail, particularly for moving bulk raw materials.

r. Arrangements for the storage of materials

s. Monitoring compliance, reporting and review - This should focus on how the site performs against the elements set out in the CLP.

t. CLP management - Details of how the CLP will be managed, including the contact details for the person who is responsible for the CLP before and during construction.

u. A CLP written in accordance with the 'London Best Practice Guidance: The control of dust and emission from construction and demolition' or relevant replacement.

The development shall be carried out in accordance with the approved construction methodology unless otherwise agreed in writing by the Local Planning Authority.

Reason: To ensure the implementation of the development does not lead to damage to the existing highway and to minimise disruption to neighbouring properties and the environment.

7. No demolition, construction or maintenance activities audible at the site boundary of any residential dwelling shall be undertaken outside the hours of 08.00 to 18.00 Monday to Friday and 08.00 to 13.00 Saturday or at any time on Sundays and bank or public holidays without the written approval of the Local Planning Authority, unless the works have been approved in advance under section 61 of the Control of Pollution Act 1974.

Reason: To minimise noise disturbance.

- 8 The development to which this permission relates must be begun not later than the expiration of three years beginning with the date of the decision notice.

Reason: To comply with the provisions of S.51 of the Planning & Compulsory Purchase Act 2004

4 Site and Surroundings

- 4.1 Falcon Road Spur is located on the southern boundary of the Alma Estate and is accessed from Falcon Road. To the south, the access road adjoins the rear of residential properties in Falcon Crescent. To the north, the spur is

adjacent to the site of the former Ponders End Youth Centre and Welcome Point Community Centre, now in the process of redevelopment to provide new replacement facilities. The end of the spur provides a drop off point for the Oasis Academy as well as providing emergency vehicular access to Dujardin Mews.

- 4.2 The existing Falcon Road Spur car park is a 37 space public car park (pay and display) with the first 15 minutes free to facilitate drop off for the Oasis Academy. Surveys carried out by the applicant in May 2017 showed that there was a clear peak in usage at around 1500 hours when the car park was at capacity, with illegal parking in the central reservation. The morning school drop off peak was less pronounced. At all other weekday times, usage was very low.

5 Proposal

- 5.1 This application is to amend the allocation of parking spaces in Falcon Road Spur approved in August 2018 (17/04816/FUL).

- 5.2 There is no change to the previously approved application 17/04816/FUL other than in respect of the allocation of parking spaces and this proposal comprises the reconfiguration of the existing car park layout to provide the following:

- Provision of 30 car park spaces for the residential units of Phase 2Aii and 9 spaces for the wider community use (including the youth centre and community centre) and associated landscaping, together with 14 drop off bays around the turning area nearest the school.
- Formation of vehicular access to the Youth Centre mechanics workshop as part of Phase 2Aii
- Retention of emergency access to Dujardin Mews
- Remodelled drop off facility for the school including a landscaped central area including a rain garden and tree planting.

- 5.3 With the construction of Phase 2Ai and 2Aii well advanced, an occupier for the ground floor medical centre unit in Phase 2Ai in Woodall Road has been secured. The proposed operator of the medical centre has requested additional car parking spaces to meet their operational needs. They have indicated that additional car parking spaces are required beyond the four designated at outline planning stage located outside the unit on Woodall Road. This application has therefore been submitted to reallocate spaces on Falcon Road Spur to facilitate medical centre staff parking.

- 5.4 It is proposed to reallocate 10 spaces in Falcon Road Spur for medical centre Use - 5 from the residential uses and 5 from the community uses. The layout would remain as approved, retaining the overall number of spaces (49). The drop off zone in the turning circle outside the school remains unchanged and will continue to provide an arrangement to allow drop-off adjacent to the school entrance.

- 5.5 The parking spaces for the community centre/youth centre use will reduce from 14 to 9. However, it is also proposed to make these 5 spaces reallocated from the community use to the medical centre available for school drop off and pick up during 08.30 to 9.00 in the morning and 15.00 to 15.30 in the afternoons. This has been agreed by the medical centre and the school.

- 5.6 The 10 spaces for the medical centre are required to support the operation of the essential service. The medical centre will be operated by Medicus which is the largest GP super partnership in London. They have 14 sites across Enfield and as a result, in addition to meeting local need, effectively the whole borough is the catchment area. It is envisaged however that for this facility, patients will predominantly be from EN3. It is an NHS organisation and is open to NHS patients. The medical centre will have 20 full time staff, including 10 on-call doctors who will undertake emergency visits during the day with travel required by car. There will be an operating theatre on site, which will require specialised staff and surgeons to visit. There will also be a pharmacy on site. In addition to emergency visits, the centre will employ paramedics and clinical pharmacists who will be doing home visits during the day for medication reviews and supporting the old and frail who are not able visit the practice.
- 5.7 The centre will operate between 8am-8pm Monday to Saturday and Sunday 9am-6pm. It will serve 10,000 registered patients. Whilst difficult to predict, particularly in current COVID-19 circumstances, the expected patient foot fall at a practice of this size is c.300 per day.
- 5.8 In order to make sure that the spaces are available for the designated users and for school drop off and pick up, it is proposed that the medical centre bays will be controlled by Traffic Order restrictions between 9am and 3pm for an Falcon Road Spur specific Controlled Parking Zone (CPZ). This will allow the school drop off to occur outside of these hours and also prevent all day parking. As the Medical Centre is due to open 7 days a week, the CPZ will align with this. It is proposed that a review of the CPZ be undertaken after 6 months occupation to ensure that the operation is effective. This is will be secured by planning obligation. (Please see extract from Transport Assessment below for details of the proposed car park management strategy).

Table 2 Proposed Management Strategy

FRS Space	Management and Enforcing Authority	Enforcement
Residential Bays	LBE Estate Management through parking wardens and tickets, information through signage.	<ul style="list-style-type: none"> ■ Bollards ■ Permits ■ Road markings stating 'Private Parking' ■ Signage to state 'Private Residential Parking only'
Community Centre Bays	LBE/ Community Centre staff/ School (for drop off spaces) through signage	<ul style="list-style-type: none"> ■ Bollards ■ Permits ■ Signage to state 'Community Centre parking only between 0900 to 1500'
Medical Centre	Medical Centre and School for shared drop off spaces through parking warden/ school staff and signage	<ul style="list-style-type: none"> ■ Road markings ■ Bollards ■ Signage to state 'Medical Centre parking only between 0900 to 1500 and 1530 to 1830'
Drop Off Area	School, through designated school personnel	<ul style="list-style-type: none"> ■ LBE Parking ■ Ticketing ■ Signage to state 'Drop off and Pick up only at all times – No Parking'
Drop Off Area – Mechanics Workshop Access	Occupiers of Mechanics Workshop	<ul style="list-style-type: none"> ■ Gated or similar
Drop Off Area – Dujardin Mews Access	Dujardin Mews development	<ul style="list-style-type: none"> ■ Barrier system to the south

6 Relevant Planning Decisions

- 6.1 Outline permission was granted in June 2017 for the Alma Estate regeneration Master Plan (15/02039/OUT) and was amended by a S73 permission 17/04670/VAR dated 17 August 2018. Full planning permission was granted for Phase 1A of the Alma Estate (15/02040/FUL) in March 2017 and Reserved Matter Approval for Phases 2A, 2Ai and 2Aii on 17 August 2018. Phase 2Ai contained space for a medical centre for which the reconfiguration of the car parking in Falcon Road Spur proposed by an accompanying application (see below) was approved at the same time.
- 6.2 Planning permission was granted in August 2018 (ref. 17/04816/FUL) for the reconfiguration and extension of existing car park to provide a total of 35 spaces for residential use, a total of 14 spaces for community uses and a drop off zone (14 spaces) to be provided by Phase 2Aii of the Alma Estate redevelopment (15/02039/OUT and 17/04748/RM) including formation of vehicular access to youth centre and alterations to school drop off point

7. Consultations

7.1 Public Response

Consultation letters were issued to 46 neighbouring and nearby properties. Furthermore, 3 site notices were posted and surrounding residents have been consulted. No comments have been received.

7.2 Internal

Traffic and Transportation

- 7.2.1 No objections subject to amendments to the S106 agreement to secure the management arrangements and funding for the parking controls proposed.

Officer Comment – these provisions will be secured through amendment to the S106 agreement.

Waste Management

- 7.2.2 No Comment.

Suds Team

- 7.2.3 A condition is proposed to require the submission of sustainable drainage proposals.

Housing Development and Estate Regeneration

- 7.2.4 The Regeneration Team support the reallocation of parking spaces for the community centre/youth centre from 14 to 9 in order to accommodate 50% of the 10 spaces required by the medical centre to operate a large GP practice from their adjacent premises.
- 7.2.5 They consider that the proposed CPZ (specifically for Falcon Road Spur) between the hours of 9am to 3pm will prevent the spaces being used by commuters or businesses whilst still allowing enough flexibility to act, when vacant, as short term overspill spaces in addition to the drop off spaces located around loop in front of the school's nursery entrance as and when the need should arise.
- 7.2.6 They consider that 9 spaces for the community centre and youth centre should provide adequate availability of spaces for essential staff serving these facilities, with the majority of service users attending via foot or public transport.
- 7.2.7 In line with the Council's climate emergency declaration and commitment to healthy streets, it is our hope that both the medical centre and community/youth centre will work to reduce the reliance on car use over the coming years and we strongly encourage all those involved with the management of these buildings to commit to doing so. However, in recognition that this cannot happen overnight and the importance of enabling a medical centre to open in this location, we are happy to support the application as it currently stands.

Officer Comment - the requirement to reduce reliance on the car will be addressed through an obligation in the S106 agreement to submit a travel plan for the medical centre staff and users.

8 Relevant Policy

8.1 The London Plan 2016

The London Plan is the overall strategic plan for London setting out an integrated economic, environmental, transport and social framework for the development of London for the next 20-25 years. The follow policies are considered relevant to the consideration of this application:

Policy 2.6 – Outer London: vision and strategy
Policy 2.7 – Outer London: economy
Policy 2.8 – Outer London: transport
Policy 2.14 – Areas for regeneration
Policy 3.1 – Ensuring equal life chances for all
Policy 3.2 – Improving health and addressing health inequalities
Policy 3.16 – Protection and enhancement of social infrastructure
Policy 3.17 – Health and social care facilities
Policy 4.12 – Improving opportunities for all
Policy 5.1 – Climate change mitigation
Policy 5.2 – Minimising carbon dioxide emissions
Policy 5.3 – Sustainable design and construction
Policy 5.10 – Urban greening
Policy 5.12 – Flood risk management
Policy 5.13 – Sustainable drainage
Policy 5.18 – Construction, excavation and demolition waste
Policy 5.21 – Contaminated land
Policy 6.9 – Cycling
Policy 6.10 – Walking
Policy 6.12 – Road network capacity
Policy 6.13 – Parking
Policy 7.1 – Lifetime neighbourhoods
Policy 7.2 – An inclusive environment
Policy 7.3 – Designing out crime
Policy 7.5 – Public realm
Policy 7.14 – Improving air quality
Policy 7.15 – Reducing noise and enhancing soundscapes
Policy 7.19 – Biodiversity and access to nature
Policy 7.21 – Trees and woodlands

8.2 The London Plan – Intent to Publish Version - December 2019

8.2.1 The Intend to Publish London Plan was published on 9 December 2019. The Secretary of State for Housing, Communities and Local Government has responded and directed that the Plan cannot be published until the Directions he has listed are addressed. He has raised concerns that there were a number of inconsistencies with national policy and missed opportunities to increase housing delivery. In the circumstances, it is only those policies of the Intention to Publish version of the London Plan, that remain unchallenged to which weight can be attributed.

8.2.2 Although there are a number of proposed changes from the London Plan 2016 of relevance to this application, none of these proposed changes would result in a different conclusion in relation to this application. Of relevance are:

- GG6 – Increasing efficiency and resilience – supports the move towards a low carbon circular economy contributing towards London becoming a zero-carbon city by 2050. Buildings and infrastructure should be designed to adapt to a changing climate, make efficient use of water and reduce impacts from natural hazards like flooding and heatwaves
- D 8 – Public Realm – development proposals should encourage and explore opportunities to create new public realm where appropriate. The quality of public

realm has a significant influence on quality of life and should be seen as a series on connected routes and spaces.

- SI 13 – Sustainable drainage – development proposals should aim to achieve greenfield run-off rates and ensure that surface water run-off is managed as close to its source as possible. There should be a preference for green over grey features in line with the drainage hierarchy.
- T1 – Strategic approach to transport – development proposals should facilitate the delivery of the Mayor’s strategic target of 80% of all trips in London to be made by foot, cycle or public transport by 2041.
- T2 – Healthy Streets – development proposals should demonstrate how they will deliver improvements that support the ten Healthy Streets indicators in line with Transport for London guidance.
- T5 – Cycling – development proposals should help remove barriers to cycling and create a healthy environment in which people choose to cycle. This includes support for London wide cycle routes and minimum standards for cycle parking.
- T6 – Car Parking – car parking should be restricted in line with levels of existing and future public transport accessibility and connectivity. Car free developments should be the starting point for all development proposals in places well connected to public transport with developments elsewhere designed to provide the minimum necessary parking. Maximum car parking standards set out in Policy T6.1 (Residential) and T6.5 (Non-residential) should be applied. Sufficient electric car charging points should be provided in new developments.

8.3 Local Plan – Core Strategy

Core Policy 1: Strategic growth areas

Core Policy 8: Education

Core Policy 9: Supporting Community Cohesion

Core Policy 21: Delivering sustainable water supply, drainage and sewerage infrastructure

Core Policy 24: The road network

Core Policy 25: Pedestrians and cyclists

Core Policy 26: Public transport

Core Policy 28: Managing flood risk through development

Core Policy 29: Flood management infrastructure

Core Policy 30: Maintaining and improving the quality of the built and open environment

Core Policy 32: Pollution

Core Policy 36: Biodiversity

Core Policy 40: North East Enfield

8.4 Development Management Document

DMD 16: Provision of new community facilities

DMD 17: Protection of community facilities

DMD 18: Early Years Provision

DMD45: Parking Standards and Layout

DMD47: New Road, Access and Servicing

DMD48: Transport Assessments

DMD49: Sustainable Design and Construction Statements
DMD57: Responsible Sourcing of Materials, Waste Minimisation and Green Procurement
DMD59: Avoiding and Reducing Flood Risk
DMD 60: Assessing Flood Risk
DMD 61: Managing surface water
DMD 62: Flood control and mitigation measures
DMD 63: Protection and improvement of watercourses and flood defences
DMD64: Pollution Control and Assessment
DMD65: Air Quality
DMD 66: Land contamination and instability
DMD68: Noise
DMD69: Light Pollution
DMD 76: Wildlife corridors
DMD 77: Green chains
DMD 78: Nature conservation
DMD79: Ecological Enhancements
DMD80: Trees on development sites
DMD81: Landscaping

8.5 North East Enfield Area Action Plan

The NEEAAP was adopted on 8 June 2016. The following policies are of relevance:

- 4.2 Improving the quality of the pedestrian and cycling environment
- 5.3 Improving the public realm
- 7.1 Providing community facilities
- 11.1 South Street Area
- 11.2 Alma Estate regeneration
- 11.3 Ponders End station

8.6 National Planning Policy Framework (19 February 2019)

8.6.1 The National Planning Policy Framework sets out the Government's planning policies for England and how these should be applied. It provides a framework within which locally prepared plans for housing and other developments can be produced. The National Planning Policy Framework (NPPF) introduces a presumption in favour of sustainable development. For decision taking this means:

- approving development proposals that accord with the development plan without delay; and
- where the development plan is absent, silent or relevant policies are out of date, granting permission unless:

Any adverse impacts of doing so would significantly and demonstrably outweigh the benefits, when assessed against the policies in the Framework taken as a whole; or specific policies in the Framework indicate development should be restricted.

8.6.2 The NPPF recognizes that planning law requires that applications for planning permission must be determined in accordance with the development plan unless material considerations indicate otherwise. The NPPF does not change the statutory status of the development plan as the starting point for decision making.

8.6.3 Paragraph 124 of the NPPF states that the creation of high-quality buildings and places is fundamental to what the planning and development process should achieve. Good design is a key aspect of sustainable development, creates better places in which to live and work and helps make development acceptable to communities.

8.7 National Planning Practice Guidance (October 2019)

This sets out the government's planning policies and how these are expected to be applied.

8.8 Other Material Considerations

S106 SPD (2016)

Manual for Streets 1 & 2, Inclusive Mobility 2005 (DfT)

GLA: London Sustainable Design and Construction SPG (2014)

GLA: Shaping Neighbourhoods: Character and Context SPG (2014)

GLA: The Control of Dust and Emissions during Construction and Demolition SPG (2014)

GLA: Accessible London: Achieving an Inclusive Environment SPG (2014)

9 Analysis

9.1 The layout and design of the Falcon Road Spur parking and drop off areas as set out in this application, were previously approved in August 2018. The change proposed is to the allocation of spaces only. If this application is approved the total number of spaces will remain the same (49) but the number of residential spaces will be reduced from 35 to 30 and the number of community/youth centre spaces will be reduced from 14 to 9. This reallocation of spaces is considered acceptable.

9.2. It is accepted that national and local planning policy discourages the use of the private car and encourages the use of public transport wherever practical. In this case, it is considered that the efficient operation of the medical centre requires dedicated parking to be of maximum benefit to the local community. Were staff to be required to use public transport, then either the number of home visits or the number of patients seen each day would have to be reduced to accommodate staff travelling time.

9.3 It is proposed that the spaces are linked to the medical centre use, and in the event the medical centre ceases to operate, the use of the spaces does not automatically transfer to an alternative / future use. This will be secured by planning obligation which will require that should the medical centre use cease the spaces will become general community spaces and managed through the CPZ process .

9.4 In order to make the fullest possible use of parking, the medical centre bays will be controlled by Traffic Order restrictions between 9am and 3pm as part of a specific Falcon Road Spur Controlled Parking Zone. This will allow the school drop off and pick up to occur outside of these hours and also prevent all day parking. As the Medical Centre is due to open 7 days a week, the CPZ will align with this. It has been agreed to include a review of the CPZ from 6 month from occupation to ensure operation is effective. The proposed CPZ and its review will be secured as a planning obligation as part of the S106 Deed of Variation.

9.5 The proposed reduction (5 spaces) in residential car parking spaces is not considered to be significant. As part of the previously consented scheme, there

were a total of 66 units in phase 2Aii and a total of 376 units in Phase 2. The parking proposals for the consented scheme were 155 spaces providing a ratio of 0.41 spaces per unit. With the proposed amendments to Falcon Road Spur, the number of spaces will reduce to 150 for 376 units reducing the parking ratio to 0.39 parking spaces per unit.

- 9.6 With reference to the comments of Traffic and Transportation, it is considered that this is a scheme which will have limited impact on the public highway and the wider transport network. The context of this is that there is no change in overall car parking provision and contributions have already been secured to provide for, if required, parking controls on the public highway in the vicinity of the site. The proposed management arrangements are broadly similar to those in the consented scheme and are considered acceptable.
- 9.7 On the specific issue of the separate controlled parking zone, whilst the parking controls might be different from other areas across the wider site (in both timing and permit management), it would be preferable to include the FRS controls in any consultation for the wider area (this is likely to include Dujardin Mews). However, given that this proposal is still subject to approval and the Council has already started drawing down S106 contributions in respect of the parking controls for other phases, it appears that this might not be feasible. Therefore, the parking controls contribution will be higher. An appropriate payment trigger will also be required as construction appears to have commenced on the development adjacent to FRS.
- 9.8 The observation from the Regeneration Team that they would encourage the medical facility to seek to reduce car parking requirements over time is welcome. A travel plan/car parking management plan will be required to facilitate this and will be secured through the S106.
- 9.9 In line with the Public Sector Equality Duty the council must have due regard to the need to eliminate discrimination and advance equality of opportunity, as set out in section 149 of the Equality Act 2010. In making this recommendation, regard has been given to the Public Sector Equality Duty and the relevant protected characteristics (age, disability, gender reassignment, pregnancy and maternity, race, religion or belief, sex, and sexual orientation). It is considered there are no implications under the terms of the Public Sector Equalities Duty arising from the reallocation of the parking spaces to the medical facility.
- 9.10 As this is a new application for the same site previously subject to a S73 application for the Alma Estate Regeneration Project in 2018, all the conditions and obligations proposed as part of the original Falcon Road Spur application (17/04816/FUL) will be re-imposed.

10.0 S106 Deed of Variation

- 10.1 A S106 Agreement formed part of the original grant of Outline Permission for the Alma Estate Regeneration – 15/02039/OUT dated 20 June 2017.
- 10.2 A Deed of Variation to the original S106 was approved as part of the S73 Variation to the original outline permission approved on 16 August 2018. This included:
- Various Highway matters including the bringing forward of the proposed CPZ and detailed highways issues outlined in this report and raised by specialist officers.

- Legal mechanisms to tie the Falcon Road Spur application (17/04816/FUL) to the S73 outline planning permission (17/04670/VAR) and occupation of units proposed within the Reserved Matter Application (17/04748/RM)
- Management arrangements for Falcon Road Spur

10.3 Should the current application be approved a further Deed of Variation will be required to:

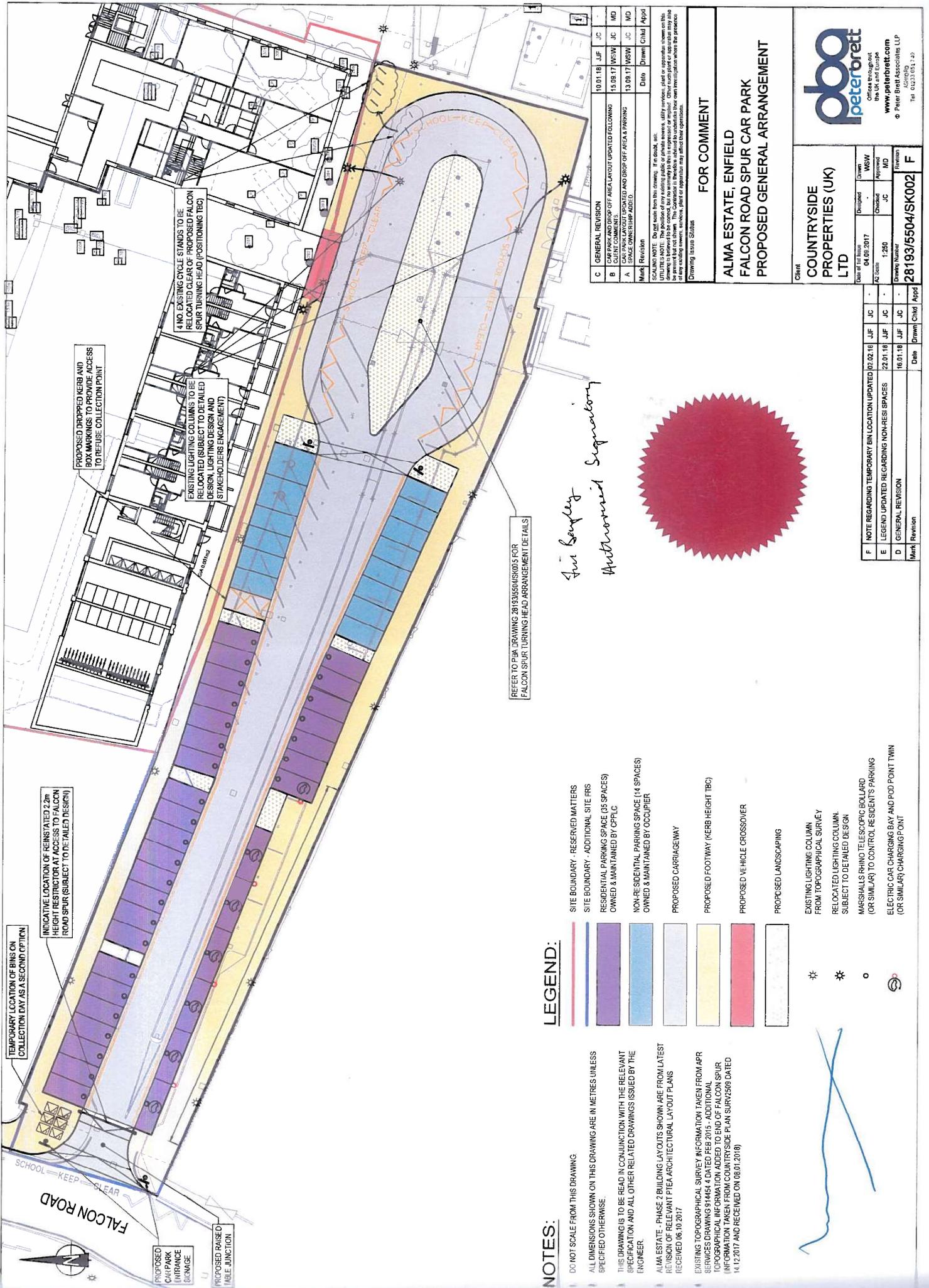
- re-affirm previously agreed obligations and to confirm minor changes to application references and users and numbers of spaces in Falcon Road Spur.
- to establish and fund a Falcon Road Spur specific Controlled Parking Zone and enable a review of arrangements after six months,
- to restrict the use of the re-allocated spaces for a medical centre use only unless the medical use shall cease in which case the spaces will be returned to general community use.
- to require a travel plan for the medical centre staff and users.

11 Conclusion

11.1 This application is a change to the allocation of car park spaces only with both the layout and number of spaces remaining as originally approved.

11.2 This proposal to reallocate parking spaces to allow dedicated spaces for medical centre staff is fully supported and will assist in the operation of this vital medical centre use. The proposals to operate a Controlled Parking Zone will make sure that the use of parking spaces is maximised.

11.3 Overall the proposals are considered to be compliant with the strategic and local planning policy frameworks.



NOTES:

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- ALMA ESTATE - PHASE 2 BUILDING LAYOUTS SHOWN ARE FROM LATEST REVISION OF RELEVANT PTEA ARCHITECTURAL LAYOUT PLANS RECEIVED 06.10.2017
- EXISTING TOPOGRAPHICAL SURVEY INFORMATION TAKEN FROM APR SERVICES DRAWING 914454.4 DATED FEB 2015 - ADDITIONAL TOPOGRAPHICAL INFORMATION ADDED TO END OF FALCON SPUR (INFORMATION TAKEN FROM COUNTRYSIDE PLAN SURV2889 DATED 14.12.2017 AND RECEIVED ON 08.01.2018)

LEGEND:

- SITE BOUNDARY - RESERVED MATTERS
- SITE BOUNDARY - ADDITIONAL SITE FRIS
- RESIDENTIAL PARKING SPACE (35 SPACES) OWNED & MAINTAINED BY CPFLC
- NON-RESIDENTIAL PARKING SPACE (14 SPACES) OWNED & MAINTAINED BY OCCUPIER
- PROPOSED CARRIAGEWAY
- PROPOSED FOOTWAY (KERB HEIGHT TBC)
- PROPOSED VEHICLE CROSSOVER
- PROPOSED LANDSCAPING
- EXISTING LIGHTING COLUMN FROM TOPOGRAPHICAL SURVEY
- RELOCATED LIGHTING COLUMN SUBJECT TO DETAILED DESIGN
- MARSHALLS RHINO TELESCOPIC BOLLARD (OR SIMILAR) TO CONTROL RESIDENT'S PARKING
- ELECTRIC CAR CHARGING BAY AND POD POINT TWIN (OR SIMILAR) CHARGING POINT

REFER TO PDA DRAWING 20130504/SK002 FOR FALCON SPUR TURNING HEAD ARRANGEMENT DETAILS

Jim Sealey
Authorised Signatory



Mark	Revision	Date	Drawn	Chkd	Appd
C	GENERAL REVISION	10.01.18	JJF	JC	
B	CAR PARK AND DROP OFF AREA LAYOUT UPDATED FOLLOWING CLIENT COMMENT	15.09.17	MSW	JC	MD
A	SPACE OWNERSHIP ADDED	13.09.17	MSW	JC	WD

FOR COMMENT

**ALMA ESTATE, ENFIELD
 FALCON ROAD SPUR CAR PARK
 PROPOSED GENERAL ARRANGEMENT**

Client
COUNTRYSIDE PROPERTIES (UK) LTD

Design: MSW
 Check: JC
 Approved: MD

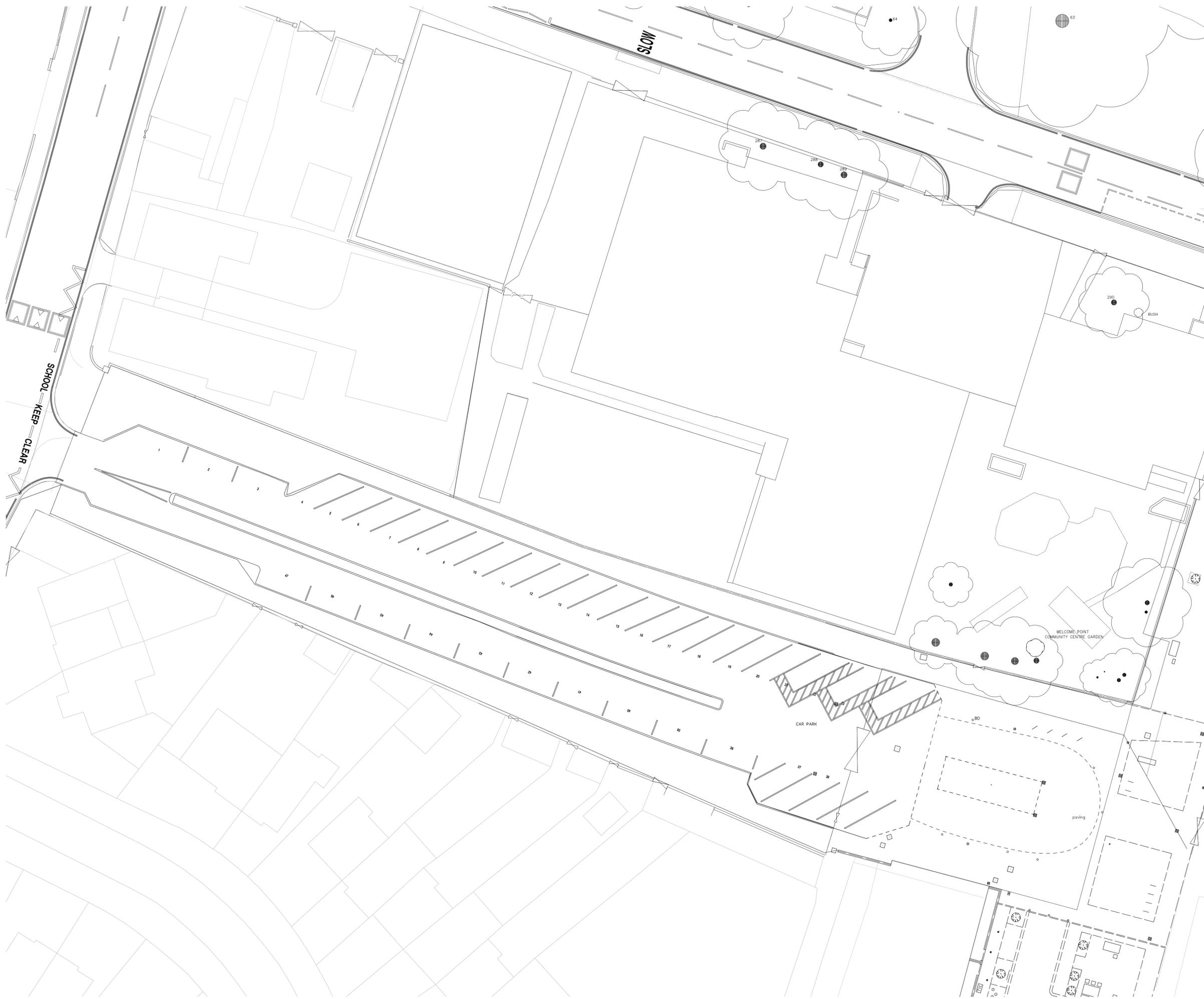
Date of 1st Issue: 04.01.2017
 AC Scale: 1:250
 Drawing Number: 28193/5504/SK002
 Revision: F

Drawn: MSW
 Chkd: JC
 Appd: MD

Scale: 1:250

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Mark	Revision	Date	Drawn	Chkd	Appd
F	NOTE REGARDING TEMPORARY BIN LOCATION UPDATED	02.02.18	JJF	JC	
E	LEGEND UPDATED REGARDING NON-RESI SPACES	22.01.18	JJF	JC	
D	GENERAL REVISION	16.01.18	JJF	JC	



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 If in doubt, ask

Page 71

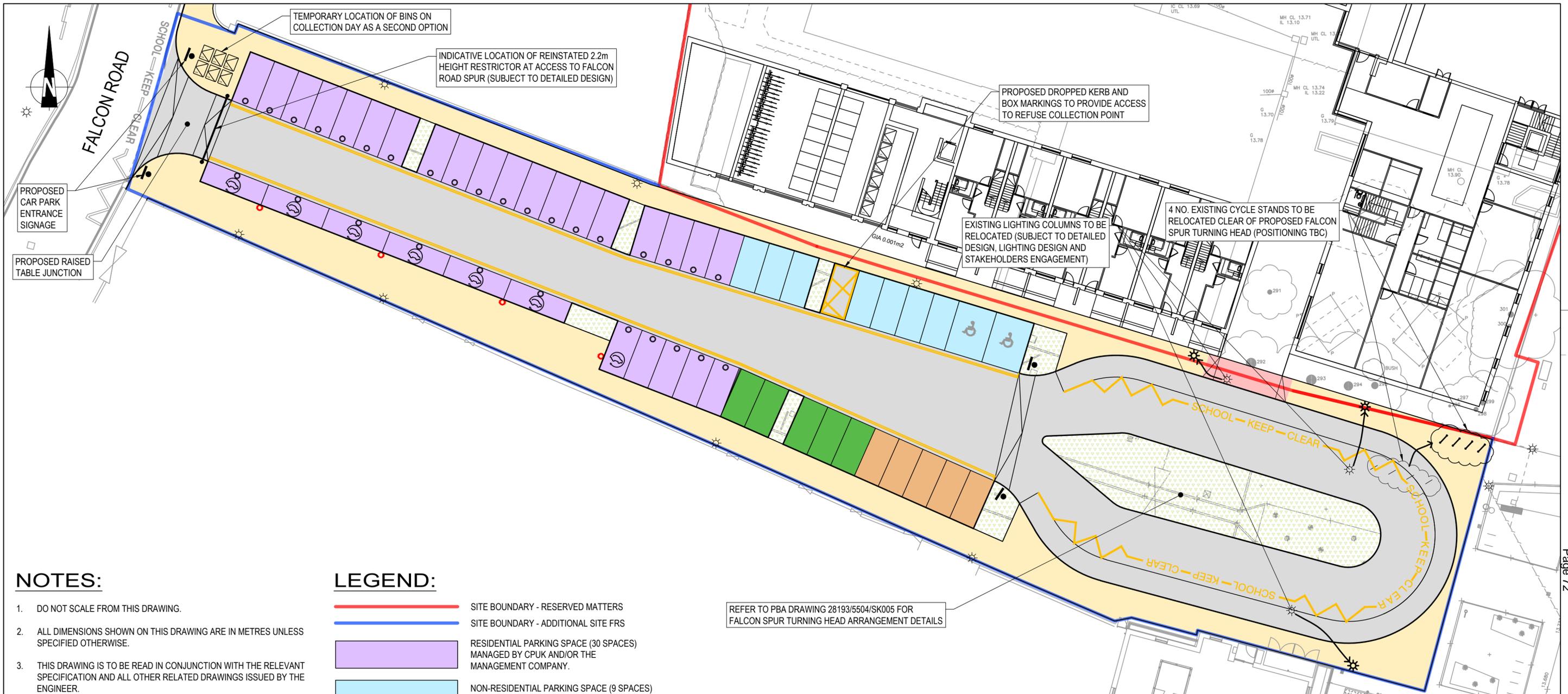
no.	date	description	drawn	checked
-	27.10.17	Planning submission	LF	LF

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**Pollard
 Thomas
 Edwards**

project	job no.	drawn	checked	scale	date
Alma Phase 2	16-084	LF	LF	1:200@A1	Oct'17
drawing title	drawing number	revision			
Falcon Road Spur Existing Layout	084_FRS_PL_100	-			



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- ALMA ESTATE - PHASE 2 BUILDING LAYOUTS SHOWN ARE FROM LATEST REVISION OF RELEVANT PTEA ARCHITECTURAL LAYOUT PLANS RECEIVED 06.10.2017
- EXISTING TOPOGRAPHICAL SURVEY INFORMATION TAKEN FROM APR SERVICES DRAWING 914454 4 DATED FEB 2015 - ADDITIONAL TOPOGRAPHICAL INFORMATION ADDED TO END OF FALCON SPUR (INFORMATION TAKEN FROM COUNTRYSIDE PLAN SURV2509 DATED 14.12.2017 AND RECEIVED ON 08.01.2018)

LEGEND:

- SITE BOUNDARY - RESERVED MATTERS
- SITE BOUNDARY - ADDITIONAL SITE FRs
- RESIDENTIAL PARKING SPACE (30 SPACES) MANAGED BY CPUK AND/OR THE MANAGEMENT COMPANY.
- NON-RESIDENTIAL PARKING SPACE (9 SPACES) MONITORED BY OCCUPIER.
- MEDICAL CENTRE PARKING (5 SPACES) MONITORED BY OCCUPIER.
- MEDICAL CENTRE SPACES ALSO USED FOR SCHOOL DROP OFF (5 SPACES) MONITORED BY OCCUPIER.
- PROPOSED CARRIAGEWAY
- PROPOSED FOOTWAY (KERB HEIGHT TBC)
- PROPOSED VEHICLE CROSSOVER
- PROPOSED LANDSCAPING
- ☼ EXISTING LIGHTING COLUMN FROM TOPOGRAPHICAL SURVEY
- ☼* RELOCATED LIGHTING COLUMN. SUBJECT TO DETAILED DESIGN
- MARSHALLS RHINO TELESCOPIC BOLLARD (OR SIMILAR) TO CONTROL RESIDENT'S PARKING
- ⚡ ELECTRIC CAR CHARGING BAY AND POD POINT TWIN (OR SIMILAR) CHARGING POINT

C	KEY WORDINGS UPDATED	19.11.20	JAD	MD	MD
B	KEY REVISED	22.10.20	REM	MD	MD
A	CHANGE THE PARKING AREA FOR SCHOOL DROP OFF	02.09.20	JAD	MD	MD
Mark	Revision	Date	Drawn	Chkd	Appd

SCALING NOTE: Do not scale from this drawing. If in doubt, ask.
 UTILITIES NOTE: The position of any existing public or private sewers, utility services, plant or apparatus shown on this drawing is believed to be correct, but no warranty to this is expressed or implied. Other such plant or apparatus may also be present but not shown. The Contractor is therefore advised to undertake their own investigation where the presence of any existing sewers, services, plant or apparatus may affect their operations.

FOR COMMENT

**ALMA ESTATE, ENFIELD
FALCON ROAD SPUR CAR PARK
PROPOSED GENERAL ARRANGEMENT**

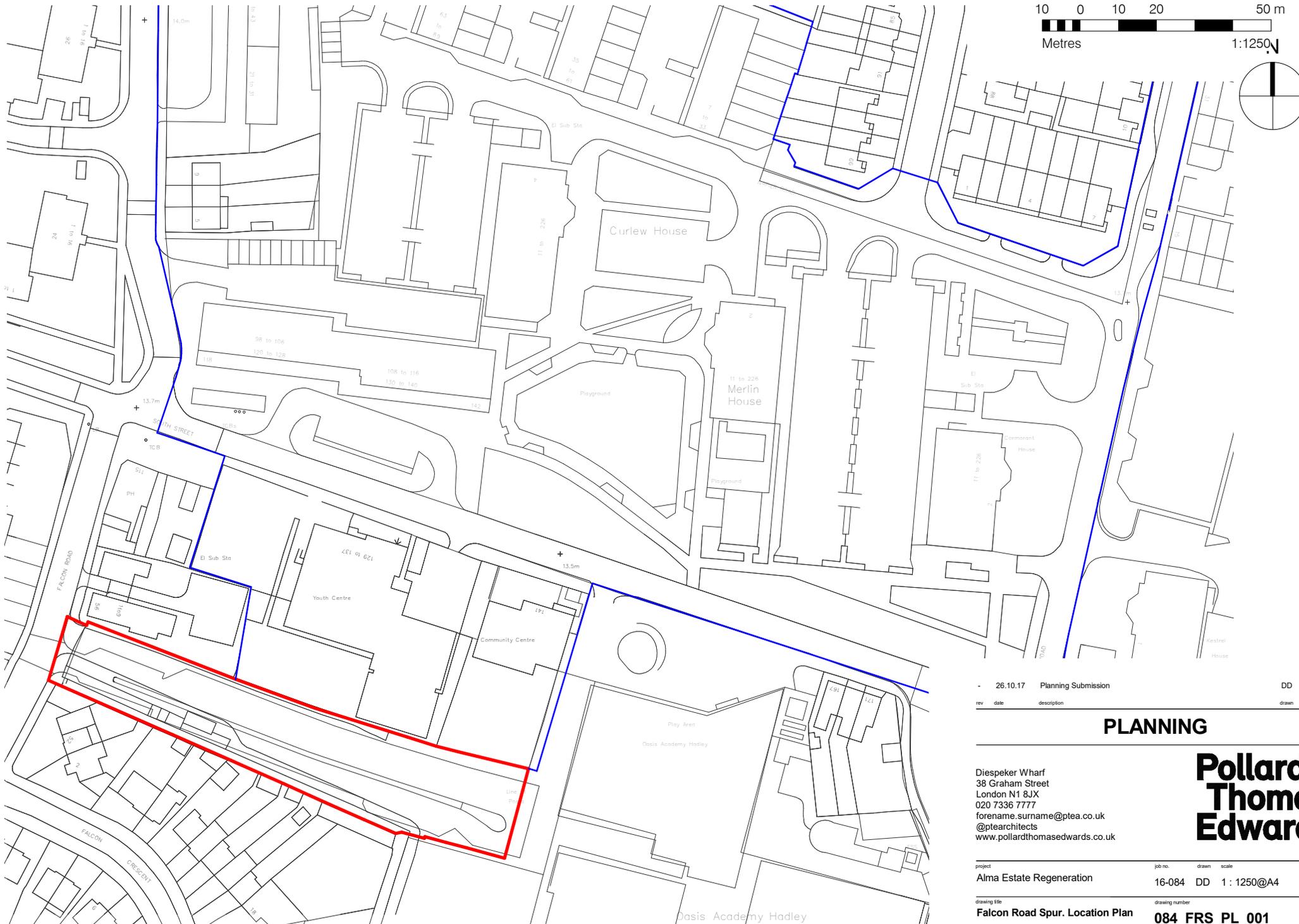
Client
COUNTRYSIDE PROPERTIES (UK) LTD

Date of 1st Issue: 07.05.2020
 Designed: -
 Drawn: JAD

1:250
 Checked: MD
 Approved: MD

Drawing Number: **46051/5501/SK001**
 Revision: **C**

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26.10.17	Planning Submission	DD	LF
rev	date	description	drawn audited

PLANNING

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**Pollard
 Thomas
 Edwards**

project	job no.	drawn	scale	date created
Alma Estate Regeneration	16-084	DD	1: 1250@A4	Oct '17
drawing title	drawing number	revision		
Falcon Road Spur. Location Plan	084_FRS_PL_001	-		

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LONDON BOROUGH OF ENFIELD

PLANNING COMMITTEE

Date: 2nd February 2021

Report of

Head of Planning

Contact Officer:

Andy Higham
David Gittens
Kate Perry 02081322322

Ward:

Highlands

Ref: 20/03404/FUL

Category: Major

LOCATION: Chase Farm Hospital, The Ridgeway, EN2 8JL

PROPOSAL: Erection of a 2-storey primary school (240 pupils) and a 2-storey secondary school (360 pupils), both for use for a temporary period of 1 academic year, together associated access, parking, hard and soft landscaping and ancillary works.

Applicant Name & Address:

Bowmer and Kirkland and the Department for Education

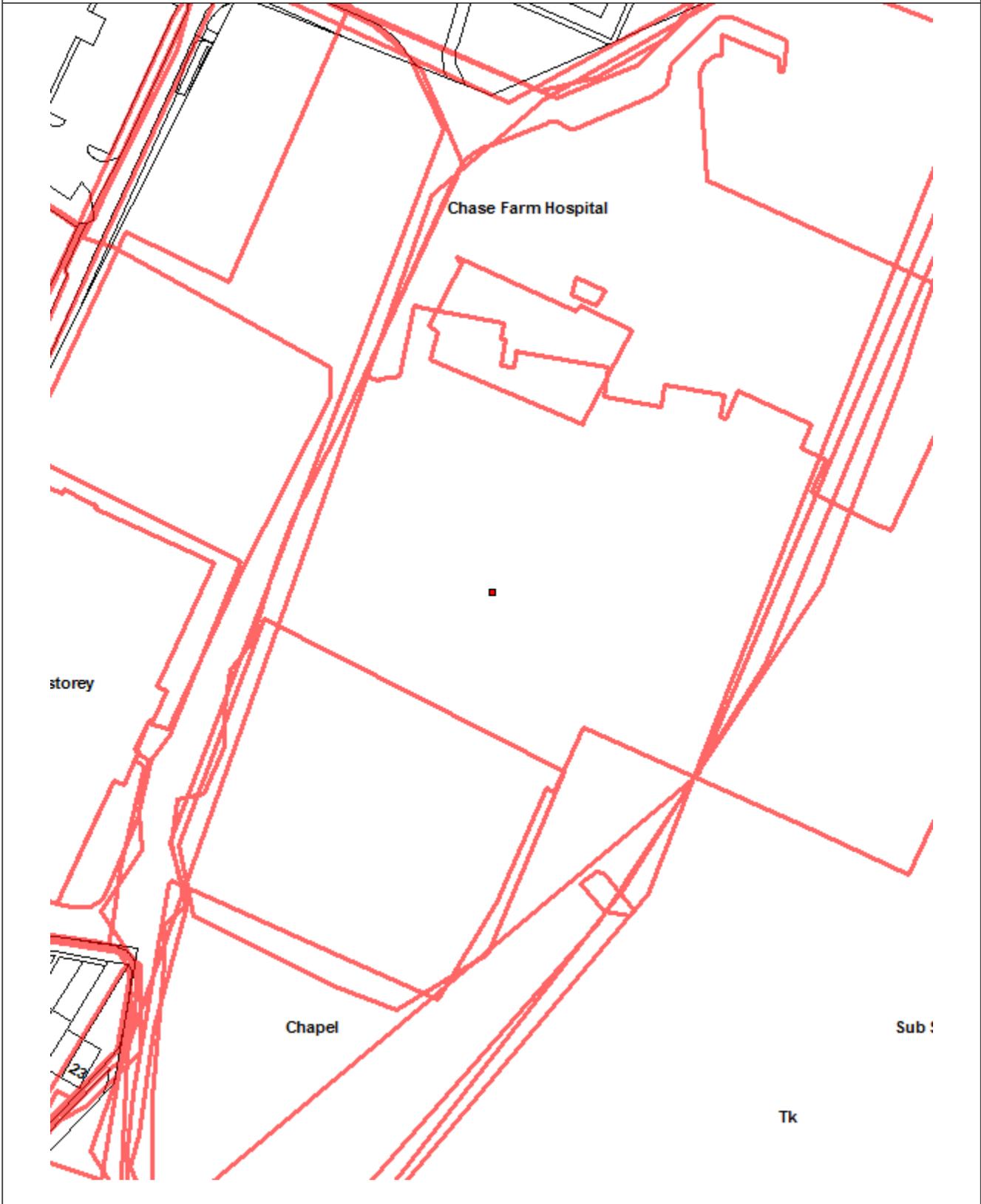
Agent Name & Address:

Mr Matthew Rhodes
DPP One Ltd
1 Park Row
Leeds
LS1 5HN

RECOMMENDATION:

That planning permission be **GRANTED** subject to conditions.

Ref: 20/03404/FUL LOCATION: Former Chase Farm Hospital (Parcel C), The Ridgeway, Enfield, EN2 8JL



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Scale 1:1250



1.0 Note for Members

1.1 This application is reported to Planning Committee because it is categorised as a “major” application. Under the scheme of delegation, this requires the proposal to be considered by the Planning Committee, where the recommendation is one of approval.

2.0 Recommendation

2.1 That planning permission be GRANTED subject to the following conditions:

- 1 This permission is granted for a limited period expiring on 30 September 2022 when the use hereby permitted shall be discontinued and/or the buildings hereby permitted removed and the land reinstated.

Reason: (i) in order to reconsider this proposal at the expiration of the limited period in the light of the circumstances then pertaining, (ii) the buildings are not considered suitable for permanent retention in view of their temporary form of construction and appearance

- 2 Unless required by any other condition attached to this Decision, the development hereby permitted shall be carried out in accordance with the following approved plans and documents:

General Arrangement	FS0404-ALA-00-XX-DR-L-0409-S2-P09
Fencing Arrangement	FS0404-ALA-00-XX-DR-L-0410-S2-P06
Secure Line	FS0404-ALA-00-XX-DR-L-0411-S2-P04
Circulation	FS0404-ALA-00-XX-DR-L-0412-S2-P03
Tree Protection Plan	FS0404-ALA-00-XX-DR-L-0413-S2-P04
Illustrative Plan	FS0404-ALA-00-XX-DR-L-0415-S2-P04
Area Calculations	FS0404-ALA-00-XX-DR-L-0416-S2-P04
Site Location Plan	FS0404-ALA-00-XX-DR-L-0417-S2-P02
Existing Site Plan	FS0404-ALA-00-XX-DR-L-0418-S2-P02
Site Section Elevations (1 of 2)	FS0404-ALA-00-XX-DR-L-0419 -S2-P02
Site Section Elevations (2 of 2)	FS0404-ALA-00-XX-DR-L-0420-S2-P02
Site Sections (1 of 2)	FS0404-ALA-00-XX-DR-L-0421-S2-P02
Site Sections (2 of 2)	FS0404-ALA-00-XX-DR-L-0422-S2-P02
Primary School Plans and Elevations	FS0404-JWA-ZZ-ZZ-DR-A-SCH001
Secondary School Plans and Elevations	FS0509-JWA-ZZ-ZZ-DR-A-SCH002
Utilities Statement	FXXXXX-CPW-00-XX-RP-N-0001
Design and Access Statement	FS0509-JWA-ZZ-XX-RP-A-2310 P01
Flood Risk Assessment	10024288/SW/001
Phase 2 Geotechnical and Geo-Environmental Ground Investigation	
Phase 1 Ground Desk Contamination Study	
Remediation Strategy	
Noise Impact Assessment	
Arboricultural Impact Assessment	RT-MME-153585-02
Arboricultural Method Statement	RT-MME-153585-03
Updated Preliminary Ecological Appraisal	
Drainage Strategy Report	076601-CUR-00-XX-RP-C-0001 P02
Archaeology Statement	
Planning Statement	
Statement of Community Involvement	
Transport Statement	

Transport Statement Update
Travel Plan
Construction Traffic Management Plan
Preliminary Arboricultural Assessment RT-MME-153585-01

Reason: In the interest of proper planning and for the avoidance of doubt.

3. Within 3 months of the occupation of the development an updated Travel Plan shall be submitted to and approved in writing by the Local Planning Authority. This shall specifically provide:
 - surveys of car park utilisation;
 - pupil travel to school mode; and
 - staff travel to work mode.

Reason: In the interests of sustainable travel

4. Prior to the occupation of the development a strategy detailing pupil pick up and drop off arrangements by car shall be submitted to and approved in writing.

Reason: In the interests of highway and pedestrian safety and to maintain the free flow of traffic.

5. The development shall be carried out in accordance with the submitted Construction Traffic Management Plan.

Reason: To ensure construction does not lead to damage of the nearby public road network and to minimise disruption to the neighbouring properties.

6. Prior to the occupation of the development, a minimum of two Electric Vehicle charging points shall be provided on the site, unless otherwise agreed in writing by the Local Planning Authority. Once installed, the Electric Vehicle charging points shall be made available for use for the lifetime of the development.

Reason: To ensure the development complies with the Council's adopted policies.

7. All areas of trees, hedges, scrub or similar vegetation where reptiles may be present, and which are to be removed as part of the development, are to be cleared during the reptile active season (between March and October).

Reason: To ensure that reptiles are protected during the period of construction.

8. If during the development evidence of reptiles are found on site, the development shall be carried out in accordance with the submitted Reptile Mitigation Strategy contained within the Preliminary Ecological Assessment, under the supervision of a suitably qualified ecologist.

Reason: To ensure that reptiles are protected during the period of construction.

9. Prior to the occupation of the development, details of proposed ecological enhancements shall be submitted to and approved in writing by the Local Planning Authority. The enhancements shall have regard to the recommendations set out in the Preliminary Ecological Appraisal.

Reason: In order to enhance the ecological value of the site.

10. The development hereby approved shall be carried out in accordance with the submitted Contamination Remediation Strategy. Prior to the occupation of the development, the Local Planning Authority shall be provided with a written warranty by the appointed specialist to confirm implementation prior to the occupation of the development.

Reason: To protect against risks arising from contamination

11. If, during development, contamination not previously identified is found to be present at the site then no further development (unless otherwise agreed in writing with the Local Planning Authority) shall be carried out until the developer has submitted, and obtained written approval from the Local Planning Authority for, an amendment to the remediation strategy detailing how this unsuspected contamination shall be dealt with.

Reason: To protect against risks arising from contamination

12. All Non-Road Mobile Machinery (NRMM) of net power of 37kW and up to and including 560kW used during the course of the demolition, site preparation and construction phases shall comply with the emission standards set out in chapter 7 of the GLA's supplementary planning guidance "Control of Dust and Emissions During Construction and Demolition" dated July 2014 (SPG), or subsequent guidance. Unless it complies with the standards set out in the SPG, no NRMM shall be on site, at any time, whether in use or not, without the prior written consent of the local planning authority. The developer shall keep an up to date list of all NRMM used during the demolition, site preparation and construction phases of the development on the online register at <https://nrmm.london/>

Reasons: In the interests of good air quality with regard to London Plan policies 5.3 and 7.14

13. Taken together, all mechanical plant across the site shall not exceed the sound pressure levels proposed in the submitted Noise Impact Assessment dated September 2020. Should proposed plant exceed the level submitted in the report details must be submitted to and approved in writing by the Local Planning Authority prior to the installation of the plant. Once approved, the plant must be installed in accordance with the approved details.

Reason: To protect the local amenity from noise and disturbance

14. No impact piling shall take place without the prior written approval of the Local Planning Authority and shall only take place in accordance with the terms of any such approval.

Reason: To protect the local amenity from noise and disturbance

15. The development shall be carried out in accordance with the details contained within the submitted Tree Protection Plan and Method Statement.

Reason: To ensure the protection and future health of retained trees.

16. Notwithstanding the details set out in the submitted Drainage Strategy Report (076601-CUR-00-XX-RP-C-0001 P02), prior to the commencement of any construction work (excluding site clearance), details of a revised Sustainable Drainage Strategy shall be submitted to and approved in writing by the Local Planning Authority and must conform with the Landscaping Strategy. The details shall include:

- A detailed drainage drawing including information on drainage runs, invert levels and cover levels, and overflows
- Location, sizes, storage volumes, cross-sections, long-sections (where appropriate) and specifications of all the source control SuDS measures including rain gardens, raised planters, green roofs, swale and permeable paving
- Final sizes, storage volumes, invert levels, cross-sections and specifications of all site control SuDS measures. Including calculations demonstrating functionality where relevant
- A management plan for future maintenance should be submitted
- Overland flow routes must be identified on the drainage drawing(s)

Reason: To ensure the sustainable management of water, minimise flood risk, minimise discharge of surface water outside of the curtilage of the property and ensure that the drainage system will remain functional throughout the lifetime of the development in accordance with Policy CP28 of the Core Strategy, DMD 61, and Policies 5.12 & 5.13 of the London Plan and the NPPF

17. Prior to occupation of the development, a Verification Report demonstrating that the approved drainage / SuDS measures have been fully implemented shall be submitted to the Local Planning Authority for approval in writing. This report must include:

- As built drawings of the sustainable drainage systems including level information (if appropriate)
- Photographs of the completed sustainable drainage systems
- Any relevant certificates from manufacturers/ suppliers of any drainage features
- A confirmation statement of the above signed by a chartered engineer

Reason: To ensure the sustainable management of water, minimise flood risk, minimise discharge of surface water outside of the curtilage of the property and ensure that the drainage system will remain functional throughout the lifetime of the development in accordance with Policy CP28 of the Core Strategy, DMD 61, and Policies 5.12 & 5.13 of the London Plan and the NPPF.

3.0 Executive Summary

- 3.1 There is an established need to deliver more school places within the Borough. A resolution to grant planning permission was agreed by Planning Committee on 20th October 2020 for the erection of a primary and secondary school, on parcels known as B2 and B3, of the Chase Farm site. Ahead of the delivery of this project, a temporary solution is required to provide school places for a period of one year (Sept 21-Sept 22) prior to the opening of the permanent schools in September 2022. Planning permission has previously been granted for a temporary secondary school on Parcel C and the current proposal would supplement this by incorporating a primary school
- 3.2 The proposal would involve the erection of a 2-storey primary school (240 pupils and 15 staff) and a 2-storey secondary school (360 pupils and 25 staff for years 7 and 8), both for use for a temporary period of 1 academic year, together with associated access, parking, hard and soft landscaping and ancillary works.
- 3.3 The reasons for recommending approval are:
- i) The principal of a temporary education use on this site has already been accepted through the grant of the earlier temporary permission for a secondary school.
 - ii) The development is needed to ensure there are enough school places available while the approved permanent schools are being built on the adjoining site (due to open September 2022)
 - iii) The proposed development due to its size and design, would appear acceptable in the street scene for a temporary period;
 - iv) The surrounding residential properties would not suffer an unreasonable loss of amenity as a consequence of the proposed development;
 - v) The proposal would provide adequate car parking, access and servicing;
 - vi) The proposal would address key sustainability initiatives in terms of its approach to drainage, energy and tree protection and, subject to conditions, is acceptable.

4.0 Site and Surroundings

- 4.1 The site is located within the Chase Farm Hospital estate. The temporary school site (identified as Parcel C) is a 1.52 ha area of land located between Chace Village Road to the north, Hunters Way to the west and Shooters Road to the east. The land was previously owned by the Royal Free Trust and formed part of the former Chase Farm Hospital campus. Planning permission has already been granted for the erection of a temporary secondary school for 184 pupils on this site.
- 4.2 The site forms part of a residential development site which has outline planning permission for the erection of up to 364 homes (granted November 2020). In addition, immediately to the north of the site on the opposite side of Chace Village Road are parcels B2 and B3. Resolution to grant planning permission was agreed for primary and secondary schools on these sites at the planning committee meeting in October 2020. The new Chase Farm Hospital is located to the north of the school sites.
- 4.3 To the immediate south of the site is an area of residential development containing a mix of retirement accommodation and semi-detached properties around Albuhera Close and Shooters Road. This area is bounded to the south by Lavender Hill. To the west of the site is a new residential development constructed by Linden Homes comprising of mostly terraced dwellings.

- 4.5 Gordon Hill railway station lies to the east of the site and a number of surrounding residential roads are subject to Controlled Parking. Overall, the site has a Public Transport Accessibility Level of 3.
- 4.6 The site is not within a Conservation Area and does not form part of the curtilage of a Listed Building. The site is subject to Tree Preservation Orders.
- 4.7 The site is not within a flood zone but is identified as being at risk of surface water flooding.

5.0 Proposal

- 5.1 The current proposal is for the erection of a 2 storey primary school (240 pupils) and a 2 storey secondary school (360 pupils – years 7 and 8), both for use for a temporary period of 1 academic year (September 2021-September 2022), together with associated access, parking, hard and soft landscaping and ancillary works. The proposal is to provide temporary school places ahead of the completion of the building of the permanent schools on parcels B2 and B3 agreed at Planning Committee in October 2020. Once the permanent schools are occupied in September 2022 the temporary buildings will be removed from the site.
- 5.2 The strategy for locating the temporary accommodation on the site has evolved as a result of various considerations and constraints; the site needs to accommodate the required number of pupil's whilst also ensuring existing tree protection and retention, and controlled access for both temporary schools. In addition, the temporary site must not restrict or block the continued ongoing construction works or the day to day activities of the neighbouring community - residents and hospital operations.
- 5.3 The temporary secondary school, Wren Academy, is proposed to the south of the site, with the primary school, One Degree Academy, to the north. It is proposed that the site would be accessed from the existing western road entry point on Chace Village Road. Relatively central to the site, and between the two schools, there would be shared car parking for 44 cars (including 5 accessible parking bays). 92 cycle parking spaces are proposed in total – 55 for the secondary school and 37 for the primary school.

6.0 Relevant Planning History

- 6.1 The whole Chase Farm site (of which parcel C forms a part) is subject to an extant outline planning permission (ref:14/04574/OUT), which approved a new hospital, a three-form entry primary school and up to 500 units of residential accommodation. The new hospital has been completed and Linden Homes have implemented the first phase of the residential element (136 units).
- 6.2 In 2019 planning permission was granted for the demolition of all buildings on site (Parcel C) and erection of a single storey secondary school (184 pupils) for use for a temporary period of 1 academic year (Sept 2020- Sept 2021), together with boundary fencing, new vehicular access from Hunters Way, associated areas of hard standing for vehicle parking and play space, together with other ancillary works.
- 6.3 Subsequently, resolution to grant planning permission was agreed in October 2020 for the demolition of buildings C3b, C3c and C3e (on parcels B2 and B3) and the erection of a new 3FE Primary School with nursery (2 storey) and a new 6FE Secondary School with sixth form (part 3 and part 4 storey), together with associated community hub, parking, highways works to provide access to the proposed schools and community hub, landscaping and outdoor sport provision (20/01997/FUL).

- 6.4 In addition, resolution to grant outline planning permission was agreed on 3rd November 2020 for the redevelopment of the site involving the demolition of buildings to provide residential units in buildings up to 6 storeys high, together with car parking, cycle parking, plant, hard and soft landscaping, and associated works (Outline Application: Access) 20/01923/OUT. Parcel C forms part of this site.

7.0 Consultations

Statutory and non-statutory consultees

Internal

- 7.1 Traffic and Transportation: No objections in principle. Conditions recommended to address outstanding issues.
- 7.2 Environmental Health: No objections subject to conditions.
- 7.3 SUDs Officer: Discussions ongoing with applicants in relation to most appropriate strategy having regard to the temporary use. Conditions recommended to ensure an appropriate solution is found.

External

- 7.5 Thames Water: No objections. Advisory directives recommended and sent to applicant.

8.0 Public

- 8.1 Consultation letters were sent to 316 neighbouring residential properties. The application was also advertised by site notice and in the local press. The consultation period ended 6.12.2020. No responses were received.

9.0 Relevant Planning Policies

9.1 London Plan (2016)

Policy 3.18	Education Facilities
Policy 5.1	Climate change mitigation
Policy 5.2	Minimising carbon dioxide emissions
Policy 5.3	Sustainable design and construction
Policy 5.7	Renewable energy
Policy 5.10	Urban greening
Policy 5.11	Green roofs and development site environs
Policy 5.12	Flood risk management
Policy 5.13	Sustainable drainage
Policy 5.14	Water quality and wastewater infrastructure
Policy 5.15	Water use and supplies
Policy 5.16	Waste self sufficiency
Policy 6.9	Cycling
Policy 6.13	Parking
Policy 7.4	Local character
Policy 7.6	Architecture
Policy 7.19	Biodiversity and Access to Nature

9.2 The London Plan – “Publication” (December 2020)

Following an Examination in Public into the submission version of the Plan and modifications, in December 2019 the Mayor published his Intend to Publish London Plan. On 13 March 2020, the Secretary of State issued Directions to change a number of proposed policies.

In response, the Mayor sent a letter to the Secretary of State on 9 December advising that he intends to approve a new draft London Plan on 21st December and send to the Secretary of State shortly after. The Mayor has advised that this will be a new, stand-alone publication version of the Plan and has been prepared to address the Secretary of State’s previous directions.

The Secretary of State has 6 weeks in which to respond (or can request a further extension of time). The Mayor can only publish the Plan after the Secretary of State has given his approval.

The Secretary of State sent a response to the Mayor on 10 December confirming amendments to the original 11 directions issued in March 2020 and issuing 2 further directions. He also invited to the Mayor to re-submit the London Plan to him. The Mayor has sent the Secretary of State the Publication London Plan December 2020.

The Secretary of State has up to 6 weeks to decide whether he is content for the Mayor to formally publish the Publication London Plan (or request a further extension of time) Once the Mayor has formally received confirmation from the Secretary of State that he is content for his Publication London Plan to be published, the Mayor will proceed with the final steps to publish his London Plan.

In line with paragraph 48 of the NPPF, the weight attached to this Plan should reflect the stage of its preparation; the extent to which there are unresolved objections to relevant policies; and the degree of consistency of the relevant policies in the emerging Plan to the NPPF.

Whilst the published London Plan (2016) remains part of Enfield’s Development Plan, given the advanced stage that the Intend to Publish version has reached, significant weight can be attached to it in the determination of planning applications (although there is greater uncertainty about those draft policies that are subject to the Secretary of State’s Direction). The following policies are considered particularly relevant:

- GG1 Building Strong and Inclusive Communities
- GG2 Making the Best Use of Land
- GG3 Creating a Healthy City
- D2 Infrastructure Requirements for Sustainable Densities
- D4 Delivering good Design
- D5 Inclusive Design
- D8 Public realm
- D11 Safety, security and resilience to emergency
- D12 Fire Safety
- D14 Noise
- S1 Delivering London’s Social Infrastructure
- S3 Education and childcare Facilities
- G6 Biodiversity and access to nature
- G7 Trees
- SI1 Improving air quality

- SI2 Minimising greenhouse gas emissions
- SI3 Energy Infrastructure
- SI5 Water Infrastructure
- SI7 Reducing Waste
- SI12 Flood Risk Management
- SI13 Sustainable Drainage
- T1 Strategic approach to transport
- T4 Assessing and Mitigating transport Impacts
- T5 Cycling
- T6 Car Parking
- T7 Deliveries, servicing and construction

9.3 Core Strategy (2010)

- CP8 Education
- CP20 Sustainable Energy use and Energy Infrastructure
- CP21 Delivering Sustainable Water Supply, Drainage and Sewerage infrastructure
- CP22 Delivering Sustainable Waste Management
- CP30 Maintaining and Improving the Quality of the Built and Open Environment
- CP34 Parks, Playing Fields and Other Open Spaces
- CP36 Biodiversity

9.4 DMD (2014)

- DMD16 Provision of New Community Facilities
- DMD37 Achieving High Quality and Design-Led Development
- DMD38 Design Process
- DMD42 Design of Civic Buildings
- DMD45 Parking Standards and Layout
- DMD47 New Roads, Access and Servicing
- DMD49 Sustainable Design and Construction Statements
- DMD51 Energy Efficiency Standards
- DMD53 Low and Zero Carbon Technology
- DMD57 Responsible Sourcing of Materials, Waste Minimisation and Green Procurement
- DMD58 Water Efficiency
- DMD59 Avoiding and Reducing Flood Risk
- DMD60 Assessing Flood Risk
- DMD61 Managing Surface Water
- DMD78 Nature Conservation
- DMD79 Ecological Enhancements
- DMD80 Trees on Development Sites
- DMD81 Landscaping

9.5 Other Material Considerations

- National Planning Policy Framework (NPPF) 2019 (revised)
- National Planning Practice Guidance (NPPG)
- National Design Guide
- Enfield Characterisation Study

10.0 Analysis

Principle of Development

10.1 Educational Need

10.2 The need for additional education facilities in this area has been accepted through the resolution to grant planning permission for the erection of a new primary school and a new secondary school on parcels B2 and B3 of the Chase Farm site. The current proposal is directly linked to the earlier permission in that it will provide much needed temporary school places for pupils whilst the permanent schools are being constructed. It is proposed that the temporary schools will open in September 2021 to provide necessary capacity for students who will then transfer to the permanent schools in September 2022.

10.3 In policy terms, the proposals are considered in the context of relevant education related policy. Policy 3.18 (Education Facilities) of the London Plan 2016 states that:

“The Mayor will support provision of childcare, primary and secondary school, and further and higher education facilities adequate to meet the demands of a growing and changing population...Development proposals which enhance education and skills provision will be supported, including new build, expansion of existing or change of use to educational purposes. Those which address the current and projected shortage of primary school places and the projected shortage of secondary school places will be particularly encouraged.”

10.4 The policy continues and states that:

“In particular, proposals for new schools, including free schools should be given positive consideration and should only be refused where there are demonstrable negative local impacts which substantially outweigh the desirability of establishing a new school and which cannot be addressed through the appropriate use of planning conditions or obligations.”

10.5 Policy 3.18 also states that:

“Development proposals which maximise the extended or multiple use of educational facilities for community or recreational use should be encouraged.”

10.6 Strategic Objective 5 of the Enfield Core Strategy 2010 (Education, health and wellbeing) seeks to ensure the capacity and quality of local social infrastructure provision, including schools, is sufficient to meet the needs of Enfield's existing population and new residents.

10.7 The proposals must also have regard to Policy 8 (Education) of the Core Strategy which seeks to contribute to improving the lives and prospects of children and young people by supporting and encouraging provision of appropriate public and private sector pre-school, school and community learning facilities to meet projected demand across Enfield. It states that new facilities should be provided on sites that offer safe and convenient access by pedestrians, cyclists and public transport users, and schools will be encouraged to allow the use of buildings for other community purposes in the evenings and at weekends.

10.8 Having regard to the above policies, it is clear that the principle of providing new or enhanced educational facilities is strongly supported in principle by adopted policy

and the strategic aims of the Council. This constitutes significant weight in favour of the proposed development when balancing the merits of the scheme.

Impact on the Character of the Area

Design

- 10.9 The NPPF (section 12) confirms that the Government attaches great importance to the design of the built environment, with good design being a key aspect of sustainable development. London Plan policies 7.4, 7.5 and 7.6 confirm the requirement for achieving the highest architectural quality, taking into consideration the local context and its contribution to that context. Design should respond to contributing towards “a positive relationship between urban structure and natural landscape features...” This is reflected in the emerging London Plan in Policies D4,D5 and D6.
- 10.10 Civic buildings are required by DMD42 to be of a high standard and prominence within their community.
- 10.11 In this instance, it is important to acknowledge that the proposed buildings are temporary in nature and will be removed from the site in September 2022. Their purpose is to provide a functional solution to provide needed school places for a temporary period. In this context, the use of 2 storey prefabricated buildings (7m high) is considered acceptable. Whilst they would not be deemed acceptable on a permanent basis, they will provide an important temporary function and will not unacceptably detract from the character of the area.
- 10.12 The temporary schools are sited to maintain the existing established vegetation and planting on the site boundaries, which assists in screening the site when viewed from the wider area.
- 10.13 Overall, the design of the buildings is considered to be an appropriate response to the setting and context of the site and, having regard to their temporary nature, will not unacceptably detract from the character and appearance of the street scene or the wider area.

Impact on Neighbouring Properties

Loss of Outlook / Light / Overlooking / Loss of Privacy / Distancing

- 10.14 The temporary schools have been sited to minimise impacts on nearby residential properties. Wren Academy (the temporary secondary school), is located closest the boundary with residential properties, being positioned adjacent to the southern boundary and in proximity to existing residential units lying to the south of the school site (Albuhera Close and Shooters Road) and the new development to the west of the site (Linden Homes). Wren Academy would be set in approximately 7m from the southern boundary of the site and approximately 14m to the western boundary, on a very similar footprint to the approved temporary secondary school. Given it would only be a maximum of 7m in height, it would not have an unacceptable impact on neighbouring residential properties in relation to loss of outlook, light or privacy detrimental to residential amenity.
- 10.15 The siting of the primary school is such that it would not be positioned in close proximity to any existing residential properties. It would be positioned towards the northern site boundary and would not have an adverse impact on residential amenity in relation to loss of light or outlook.

Noise and General Disturbance

- 10.16 Policy DMD68 states that development that would generate or would be exposed to an unacceptable level of noise will not be permitted. Where permissible, developments must be sensitively designed, managed and operated to reduce exposure to noise and noise generation. Particular regard should be given to the following:
- a. Building design;
 - b. Layout of rooms;
 - c. Positioning of building services;
 - d. Landscaping
 - e. Sound insulation;
 - f. Hours of operation and deliveries.
- 10.17 This site already has planning permission for the erection of a temporary secondary school.
- 10.18 Whilst it is acknowledged that the creation of two schools will result in a potential increase in noise and disturbance to the area due to the increased level of activity on the site, the scheme has been designed in order to minimise adverse impacts. The schools have both been positioned so that they face into the site which concentrates activity (in the form of car parking, playgrounds and pedestrian routes) centrally and away from the more sensitive boundaries with neighbouring properties. In particular, the temporary secondary school buildings will act as a barrier to noise experienced by neighbouring properties
- 10.19 It is considered that most activity would be concentrated in limited periods over the working day (mainly at drop off and pick up time) and would not extend late into the evening or occur at weekends. Weighing this relatively limited potential disturbance against the educational need for schools to support existing communities and the temporary nature of the proposal, it is considered there is no substantial concern regarding noise and general disturbance arising as a result that would weigh against the support for this proposal.

Traffic and Highway Considerations

- 10.20 Policy 6.3 of the London Plan seeks to ensure the impact of development proposals on transport capacity and the transport network are fully assessed. The proposal must therefore comply with policies relating to better streets (Policy 6.7), cycling (Policy 6.9), walking (Policy 6.10), tackling congestion (Policy 6.11), road network capacity (6.12) and parking (Policy 6.13). Policies DMD45 and 47 provide the criteria upon which developments will be assessed with regard to parking standards / layout and access / servicing.

Trip generation

- 10.21 The Transport Statement makes the case that the trips from the temporary school provision will be below the total trips for the permanent schools. Given that the permanent schools application has now been approved by Planning Committee, this assumption is accepted. As a result, given the development of the two schools already approved and the lower number of pupils / staff being accommodated, the proposal is unlikely to have any additional adverse impact on the local highway network.

- 10.22 However, it should be noted that the application for the permanent schools was supported by mitigation measures to address potential issues with active travel access and overspill parking. This is considered further in the assessment below.

Active Travel Zone Assessment

- 10.23 The applicant has used the information from the application for the permanent schools to identify improvements and recommendations in respect of the local highway network which will improve walking and cycling access to the site. Due to the need arising from this temporary proposal, it is concluded that they are not considered necessary for the temporary schools and will be delivered in line with the conditions and obligations relating to the permanent schools.

- 10.24 Given that permission is being sought for temporary accommodation for both the primary school and secondary school for a one year period from September 2021, this approach is acceptable. However, should the use of the temporary schools be extended then consideration should be given to delivery of the identified improvements and recommendations. This would be picked up if a subsequent application to extend the time period was made.

Cyclist / Pedestrian access

- 10.25 The proposed access points and routes are broadly acceptable. It has been agreed that improvements to existing pedestrian/ cycling routes will be made as part of the full planning applications for the permanent schools and residential development but that they are not required at this stage to facilitate the temporary development.

Travel Plan

- 10.26 The applicant has committed to providing a travel plan for the site. This should include undertaking surveys of car park utilisation, pupil travel to school mode and staff travel to work mode. This information can then inform the travel plans for the permanent schools, as well as provide a baseline for any extension of the duration of the temporary use.
- 10.27 The Travel Plan will be secured by way of a planning condition. This approach has been agreed to facilitate the delivery of the temporary schools within the very tight time frame available.

Vehicular Access

- 10.28 The proposal seeks to utilise the existing vehicular access from Chace Village Road. The access point will provide vehicular access for both schools. This removes the need for an access from Hunters Way and resolves an issue with the existing arrangements in respect of queuing vehicles from a core route through the wider site.

Car Parking

- 10.29 The Intend to Publish London Plan does not set maximum standards for car parking for educational institutions. Therefore, the applicant has used data from comparable sites and concluded that 44 long stay car spaces will be required for staff (including 5 accessible spaces). It is noted that this is a higher provision than would normally be expected, however the applicant has advised that the proposed quantum of car parking is necessary to meet the needs of the temporary schools where staff will be travelling from further afield and from different sites. For example, in order to operate Wren Academy it will be necessary for staff to travel between the temporary school

and an existing academy in North Finchley to meet the day to day needs of the school. Therefore, realistically car use will be essential. On the basis of the temporary nature of the use and specific needs that this generates, this is considered acceptable.

- 10.30 In respect of the drop off and pick up provision, discussions are ongoing with the Council's highways department. No provision is proposed on site at present and this was an agreed approach for the permanent schools. This is being carefully reviewed and an update provided. However, it is also recommended that this issue be covered in the Travel Plan which will be required by condition.
- 10.31 In relation to the provision of Electric Vehicle Charging points, it is advised that 2 such spaces would be adequate given the temporary nature of the provision. This will be required by condition.

Cycle Parking

- 10.32 The level of cycle parking and type exceed the higher minimum requirements as set out in the Intend to Publish London Plan, which is welcome. 85 long stay spaces and 7 short stay spaces are proposed in total (51 long stay and 4 short stay for Wren Academy and 34 long stay and 3 short stay for One Degree Academy). In addition, the plan has been amended to include an appropriately sized store to accommodate scooters within the demise of the primary school.

Delivery, Servicing, Refuse and Recycling

- 10.33 Delivery, servicing and refuse and recycling collections will take place on site. This is a positive approach as it removes vehicles from through routes and areas used by primary school pupils. At the request of the Council's Transportation Officer, vehicle tracking has been provided to show that refuse vehicles can access and circulate within the site.

Construction Traffic Management Plan

- 10.34 A Construction Traffic Management Plan has been submitted. This is acceptable and compliance can be secured (including the provision of a highway condition survey) by condition.
- 10.35 Having regard to the above, the proposal is considered to make adequate provision for access and parking in accordance with Policies 45 and 47 of the DMD.

Sustainable Design and Construction

Biodiversity / Ecology

- 10.36 Policy 7.19 of the London Plan ("Biodiversity and access to nature") requires development proposals to make a positive contribution, where possible, to the protection, enhancement, creation and management of biodiversity. Core Policy 36 of the Core Strategy confirms that all developments should be seeking to protect, restore, and enhance sites. Furthermore, Policy DMD79 advises that on-site ecological enhancements should be made where a development proposes more than 100sqm of floor space, subject to viability and feasibility.
- 10.37 An Updated Preliminary Ecological Appraisal (PEA) and reptile survey dated May 2020 have been submitted with the application. This provides an update to the ecological appraisal submitted for the previous temporary school application in May 2018.

- 10.38 The PEA acknowledges that parcel C is located in an urban environment with relatively limited connectivity to the landscape. Trees with Tree Preservation Orders (TPO) will be retained and the site boundary will retain the existing habitats (hedge with mature trees). Additional meadow grassland is proposed, providing some connecting habitat and some habitat continuity.
- 10.39 The PEA identifies that the key ecological consideration is the suitability of existing habitats for reptiles. Reptile surveys are ongoing and to date no reptiles have been recorded. Given the surveys are yet to be completed, a draft mitigation strategy has been put forward in order to demonstrate that should reptiles be found they will be adequately protected during the works. A condition is recommended that if reptiles are found the development is carried out in accordance with the submitted reptile mitigation strategy (which would involve trapping and translocation of the reptiles) under the supervision of a suitably qualified ecologist. It is recommended that the clearance is undertaken during the reptile active season between March to October). If no reptiles are identified in parcel C following the presence/likely absence surveys, no further mitigation will be necessary.
- 10.40 In terms of ecological enhancements, the PEA recommends that two reptile refugia are constructed to offer more resting and overwintering habitats. The habitats in this area can also be improved by increasing habitat diversity through the inclusion of:
- Compost heap that includes grass clippings;
 - Include hawthorn planting along the edge habitats to provide shelter from predators;
 - Ensure dense vegetation is present in addition to open areas so that reptiles can use basking habitats and also shelter from predators;
 - Maintain a diverse sward height by cutting some areas, and leaving others, to provide some habitat diversity.
- 10.41 Having regard to the above, it is considered the proposed development will not unduly impact upon the existing ecological value of the site but through measures to be secured by condition, will serve to enhance the ecological value of the site in accordance with policy 7.19 of the London Plan, CP36 of the Core Strategy and policy DMD79 of the Development Management Document.

Trees

- 10.42 Policy DMD 80 requires the retention and protection of trees of amenity and biodiversity value on a site and in adjacent sites that may be affected by proposals. Policy DMD 81 ensures development must provide high quality landscaping that enhances the local environment.
- 10.43 Tree Preservation Order No. 386 (2015) applies to several trees present on, or in close proximity to the site. The proposal seeks to retain all trees covered by the TPO.
- 10.44 The submitted Arboricultural Impact Assessment categorised trees in accordance with BS5837:2012 to establish their condition, age and quality. Category A trees are of high quality, contribute to local amenity, and should be retained if possible. Category B trees are of moderate quality with an estimated life expectancy of at least 20 years. Category C trees are of low quality, with either a limited life expectancy, or very young trees with a stem diameter of not more than 150mm, or very little contribution to local amenity. Category U trees are ones in such a poor condition that they cannot realistically be retained as living trees.

- 10.45 The proposed development will ensure the retention and incorporation of the vast majority of trees across the site. However, the proposed development will necessitate the removal of four trees and one group of trees as well as the partial removal of one group of trees. All the trees to be removed are categorised as category C or category U trees. No category A or B trees are to be removed. It is noted that the trees to be removed have been accepted for removal as part of the residential outline planning permission.
- 10.46 A condition is recommended that the development be carried out in accordance with the submitted Tree Protection Plan and Arboricultural Method Statement to ensure that the retained trees will be protected during the construction works and for the lifetime of the development.
- 10.47 In light of the above, the proposal is considered in accordance with DMD 80 and DMD 81 of the Council's Development Management Document.

Contamination

- 10.48 A Phase 1 contamination desk study was undertaken to establish potential contaminant sources and preliminary risk assessment. Based on the findings further investigation was required. This has been undertaken and the Council's Environmental Health Officer has inspected the report. It has been confirmed that the applicant will need to undertake a low level of remediation at the site: any landscaped areas will need clean soil to a depth of 30cm. The rest of the area will have hardstanding that creates a barrier between the ground contamination and the children, effectively ensuring there is no pathway for the contamination to reach the receptor.
- 10.49 In order to ensure the remediation works are carried out in accordance with the remediation report a compliance condition is recommended. In addition, a condition will require the submission of a verification report prior to the occupation of the development.
- 10.50 Conditions are also proposed to ensure there are no unacceptable impacts in relation to noise, mechanical plant emissions and impact piling.

Energy

- 10.51 The adopted policies require that new developments achieve the highest sustainable design and construction standards having regard to technical feasibility and economic viability. Normally a carbon emissions reduction of 35% beyond Part L of the 2013 Building Regulations would be expected. However, in this instance consideration has to be given to the nature of the development proposed. The application proposes prefabricated classrooms to be deposited on the site for a temporary period of 1 year. In this context, it is not proposed to make significant improvements to the energy performance of the temporary buildings. This is considered acceptable given the context. However, if a future application is made to extend the use of the buildings on the site for a longer period a requirement for energy saving measures would be considered.

SuDS

- 10.52 London Plan Policies 5.12 and 5.13 require the consideration of the effects of development on flood risk and sustainable drainage respectively. Core Policy 28 ("Managing flood risk through development") also confirms the Council's approach to flood risk, inclusive of the requirement for SuDS in all developments. Furthermore,

Policy DMD59 (“Avoiding and reducing flood risk”) confirms that new development must avoid and reduce the risk of flooding, and not increase the risks elsewhere. Consequently, planning permission will only be granted for proposals which have addressed all sources of flood risk and would not be subject to or result in unacceptable levels of flood risk on site or increase the level of flood risk to third parties.

- 10.53 In accordance with DMD61 (“Managing surface water”) a SuDS strategy has been submitted. The SUDs Officer has reviewed the strategy and notes there is significant reliance on traditional drainage features (such as gullies) which may not be suitable having regard to the temporary nature of the development. The SuDs Officer has recommended that the applicant seeks to provide a greener alternative such as swales (which are cheaper to maintain and may be easier to re-instate).
- 10.54 In order to address the above, a condition has been recommended to seek an alternative SuDs strategy prior to the commencement of development.

11. Community Infrastructure Levy (CIL)

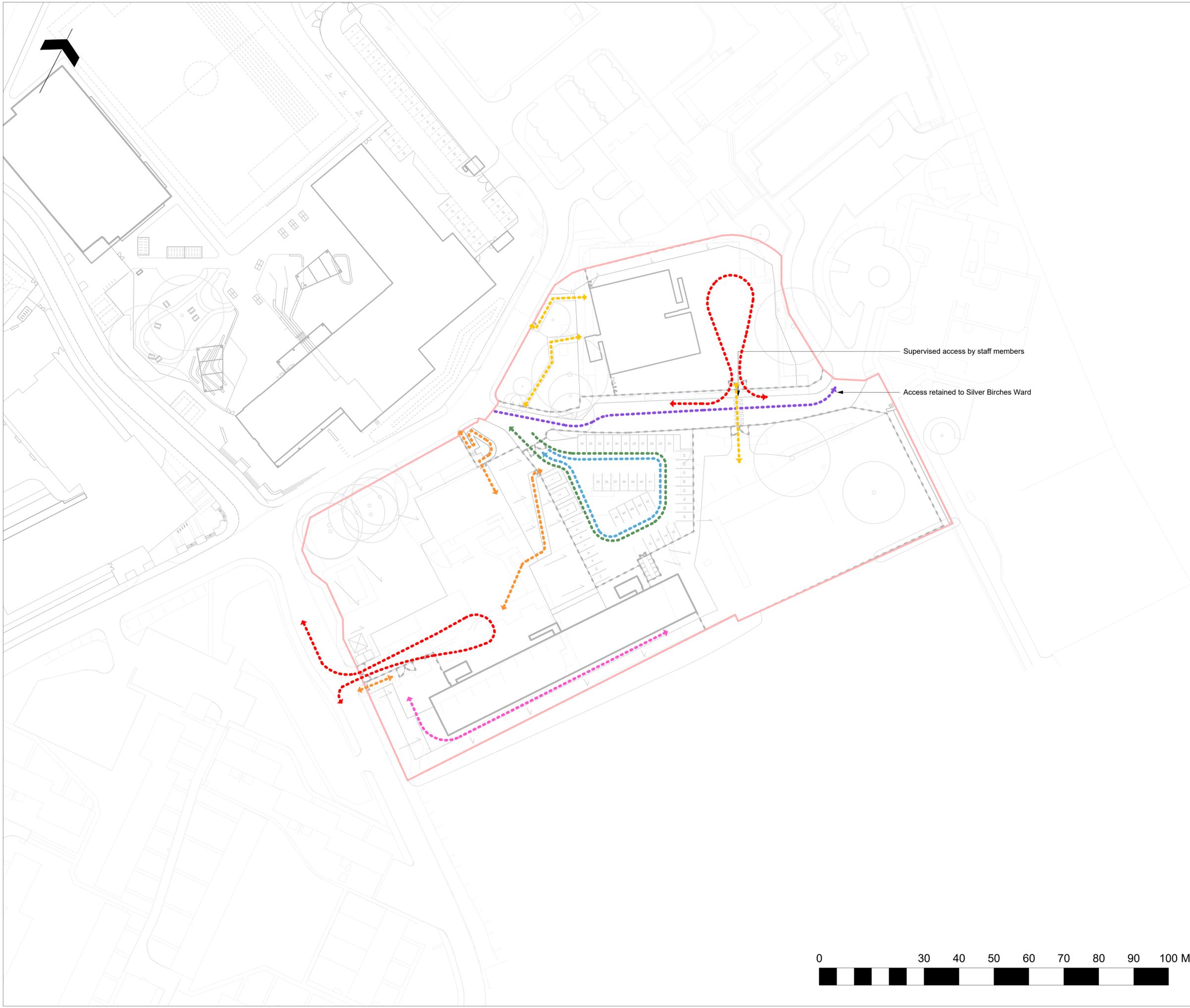
- 11.1 The proposal would not be liable for either Mayoral or Enfield CIL given the education use proposed.

12.0 Conclusion

- 12.1 Having regard to the above assessment, the use of the site to provide temporary primary and secondary schools for a period of one year is considered to be acceptable and will assist in meeting the educational needs of pupils in the Borough whilst the associated permanent schools on the adjacent land parcels are being constructed.
- 12.2 The buildings and associated works are considered appropriate given their size, form and detailed design. They would not have a detrimental impact on the character and appearance of the wider street scene particularly having regard to the temporary nature of the proposal.
- 12.3 The development by virtue of its size, location and proximity would not unacceptably harm the amenity of neighbouring and nearby residents.
- 12.4 The proposal would not cause any unacceptable harm upon highway safety or the flow of traffic in the locality and would provide adequate car parking, cycle parking and servicing arrangements.
- 12.5 The proposals are considered acceptable in terms of the relationship to issues of biodiversity, trees, contamination and sustainable drainage.
- 12.6 The development is therefore appropriate and in accordance with relevant National and Regional Policy, Core Strategy and Development policies and for the reasons noted above.



- Key**
- Vehicular drop-off access
 - Staff parking access
 - Delivery / refuse vehicle circulation
 - ODA pedestrian access
 - WREN pedestrian access
 - MEWP maintenance access
 - Fire tender access



P03	28/09/2020	Trees updated to latest AIA. Issued for Planning
P02	24/09/2020	Substation Added Issued for Planning
P01	21/09/2020	Issued for Planning
REV	DATE	DESCRIPTION

CLIENT

Department for Education

CONTRACTOR

BOWMER KIRKLAND

CONSULTANT

ares
LANDSCAPE ARCHITECTS

PROJECT

Wren Academy & One Degree Academy Temporary Site

DRAWING TITLE

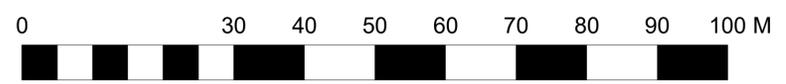
Circulation

DRAWING PURPOSE

S2 - PLANNING

DRAWN BY	DATE	CHK'D BY	SCALE	MEDIA
CC	16/06/2020	CW	NTS	A1

PROJECT - ORIGINATOR - ZONE - LEVEL - TYPE - ROLE - NUMBER	REVISION
FS0404-ALA-00-XX-DR-L-0412	P03

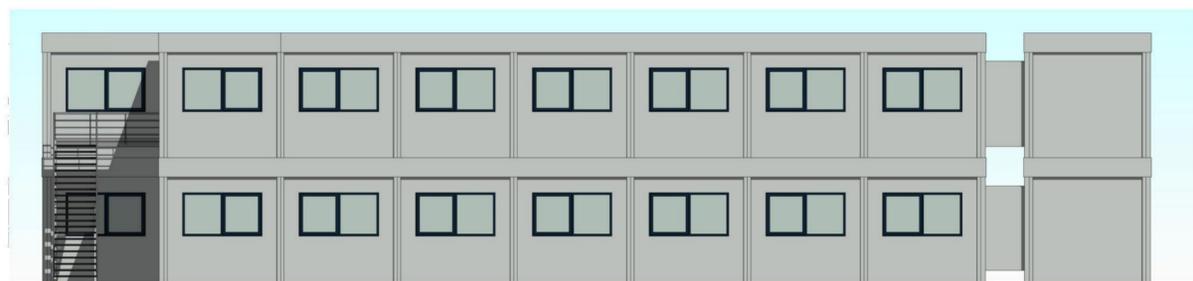


NOTES

Please note - fire exit provision and fire ratings are subject to Building Control / Fire Officer approval

Each floor 617m²
Total 1235m²

— Indicates 30 minute fire resistant wall partition



4 North Elevation
1 : 100



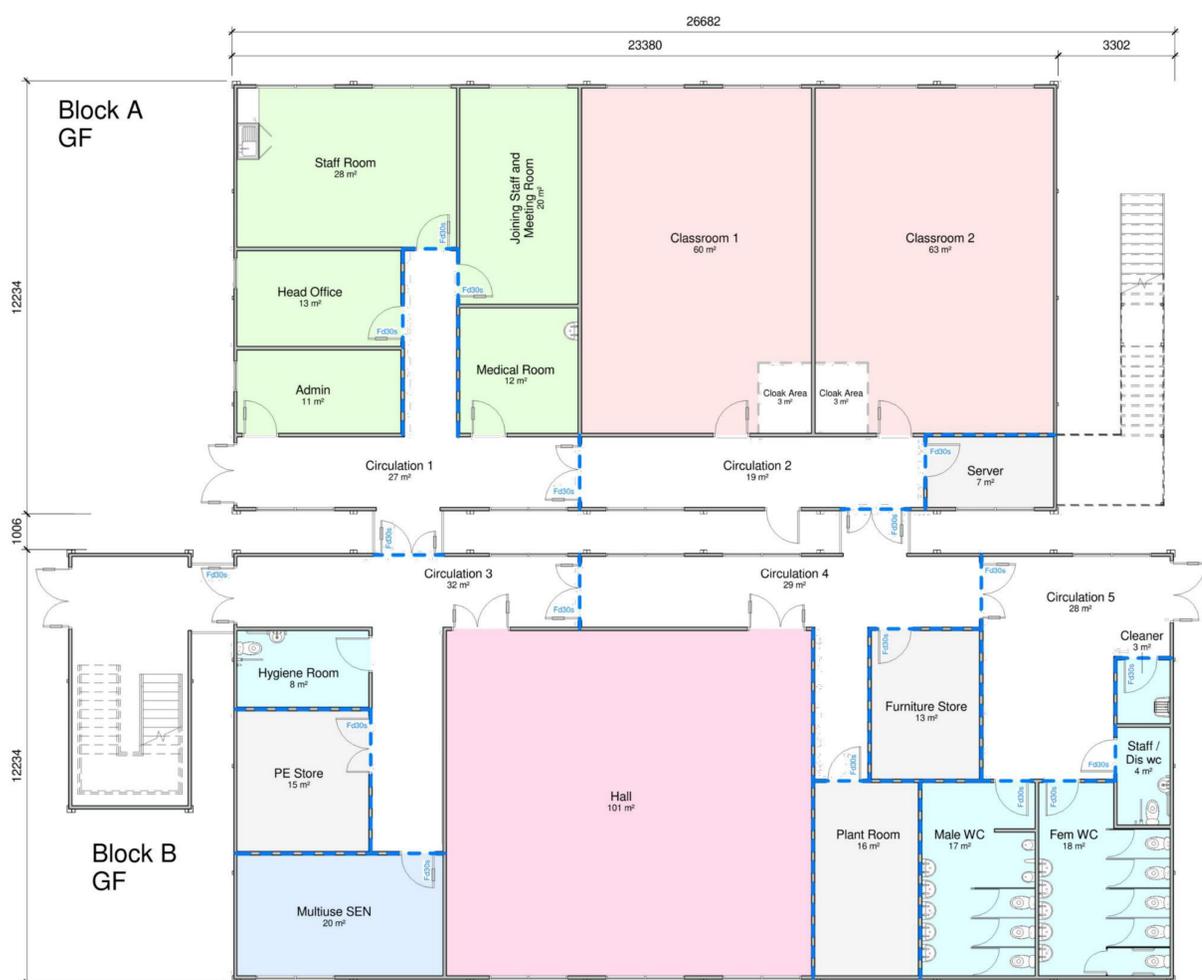
5 East Elevation
1 : 100



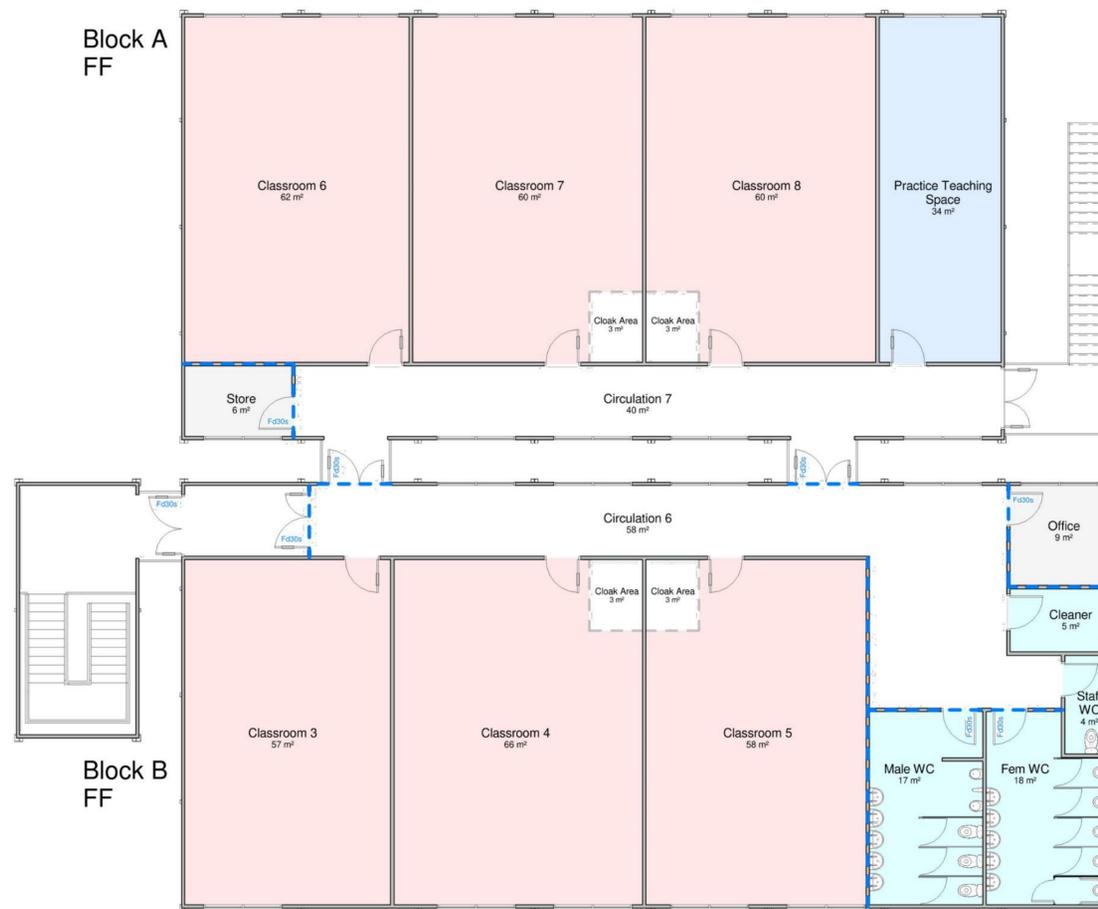
3 South Elevation
1 : 100



6 West Elevation
1 : 100



1 Ground Floor
1 : 100



2 First Floor
1 : 100

EXTERNAL COLOUR SCHEME | Ultima

Walls - Gorsewing Grey - nearest BS ref 10 A 05
Vertical wall trim - Gorsewing Grey - nearest BS ref 10 A 05
Bottom wall trim - White - nearest BS ref 00 E 55
Long wall fascias (covering wall) - Gorsewing Grey - nearest BS ref 10 A 05
Long wall fascias (covering beam) - White - nearest BS ref 00 E 55
Short wall composite fascias - Gorsewing Grey - nearest BS ref 10 A 05
Roof - White - nearest BS ref 00 E 55
External Skirt - Gorsewing Grey - nearest BS ref 10 A 05
Columns, external corners - Grey - BS 4800 00 A 05
Columns, intermediate - Grey - BS 4800 00 A 05
Windows - Blue Grey - nearest BS 4800 18 B 29
Doors (insulated door) - Slate grey - nearest BS 18 B 25

REV	DATE	DESCRIPTION	AB	MG	DRWN	CHKD
P01	30/09/20	FOR PLANNING				

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london + prague

CLIENT

BOWMER & KIRKLAND

PROJECT
Chase Farm Temporary
Accommodation

DRAWING TITLE
Proposed Layout and Elevations
One Degree Academy Temporary
Buildings

SCALE SHEET

AS INDICATED A1

DRAWING NO. REV

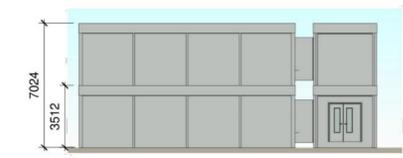
FS0404-JWA-ZZ-ZZ-DR-A-SCH001 P01

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Please note - fire exit provision and fire ratings are subject to Building Control / Fire Officer approval

Each floor 990m²
Total 1980m²

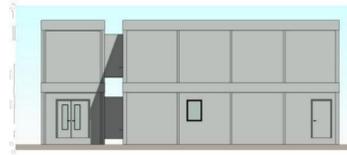
— Indicates 30 minute fire resistant wall partition



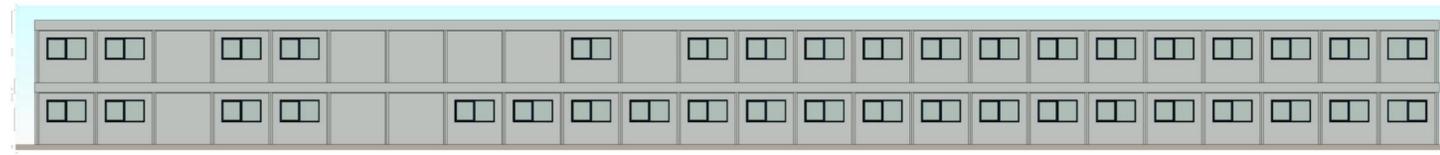
3 East Elevation
1 : 200



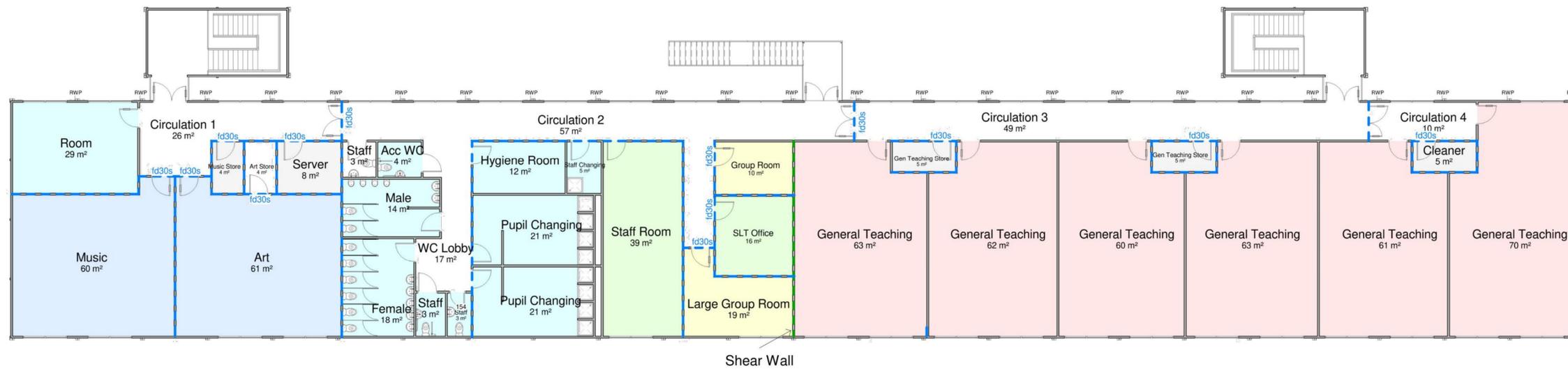
4 North Elevation
1 : 200



5 West Elevation
1 : 200

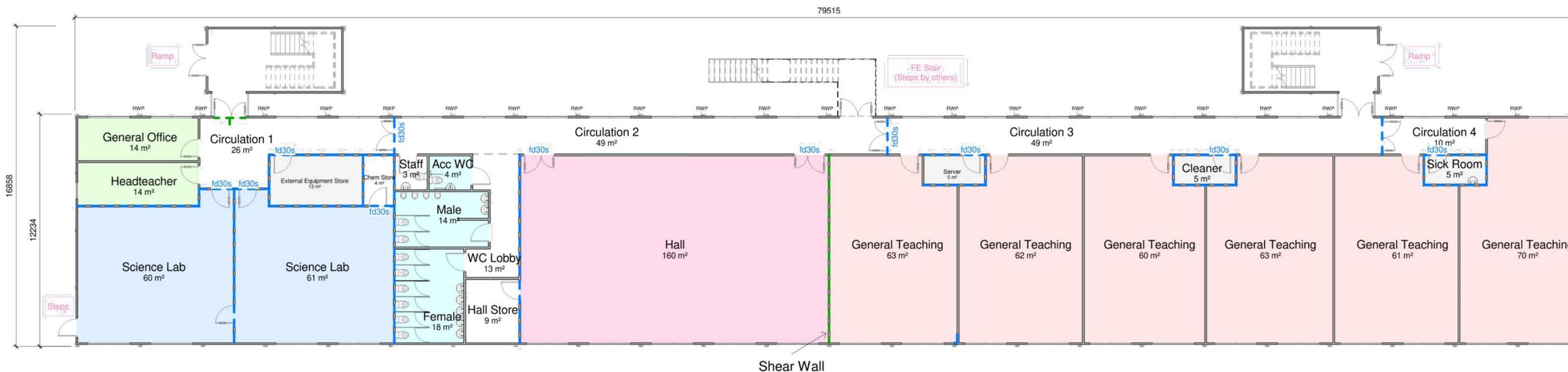


6 South Elevation
1 : 200



2 First Floor
1 : 125

- Walls - Goosewing Grey - nearest BS ref 10 A 05
- Vertical wall trim - Goosewing Grey - nearest BS ref 10 A 05
- Bottom wall trims - White - nearest BS ref 00 E 55
- Long wall fascias (covering wall) - Goosewing Grey - nearest BS ref 10 A 05
- Long wall fascias (covering beam) - White - nearest BS ref 00 E 55
- Short wall composite fascias - Goosewing Grey - nearest BS ref 10 A 05
- Roof - White - nearest BS ref 00 E 55
- External Skirt - Goosewing Grey - nearest BS ref 10 A 05
- Columns, external corners - Grey - BS 4800 00 A 05
- Columns, intermediate - Grey - BS 4800 00 A 05
- Windows - Blue Grey - nearest BS 4800 18 B 29
- Doors (insulated door) - Slate Grey - nearest BS 18 B 25



1 Ground Floor
1 : 125

REV	DATE	DESCRIPTION	AB	MG	DRWN	CHKD
P01	30/09/20	FOR PLANNING				

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CLIENT
BOWMER & KIRKLAND
PROJECT
Chase Farm Temporary
Accommodation

DRAWING TITLE
Proposed Layout and Elevations
Wren Academy Temporary Buildings



- Site**
- Site Boundary
 - Proposed Building
- Planting**
- Existing Tree Retained
 - Existing Planting
 - Existing Grass
 - Proposed Grass Turf
 - Proposed Meadow Seed to Banks
- Paving**
- Pedestrian Tarmac To Engineer's Specification
 - Vehicular Tarmac To Engineer's Specification
 - Existing Pedestrian Tarmac Retained instu
 - Existing Vehicular Tarmac Retained instu
- Fencing**
- Proposed Fencing Refer to Fencing General Arrangement for details
 - Proposed Gates Refer to Fencing General Arrangement for details
- Structures**
- Cycle stands - Kensington Systems Sheffield Type Stand (or similar)
 - Door Protection Hoop - Kensington Systems Elk Door Restrainer (or similar)
 - Cycle Shelter - Broxap New Sheffield Cycle Shelter (or similar)
 - Proposed Bin Store Refer to Fencing General Arrangement for details

One Degree Academy Accommodation

Existing Footpath

ODA cycle spaces: 3no. short stay cycle spaces

Existing Footpath

ODA cycle shelters: 34no. cycle spaces

Existing Steps, Handrails, Tarmac and Ramp Access Retained

Existing TPO Trees Retained

Wren cycle shelters: 51no. long stay cycle spaces.

Proposed Substation

Existing Access

Wren cycle stands: 4no. short cycle spaces

Wren Academy Temporary Accommodation

Proposed grass embankment

Existing pedestrian footpath retained

Proposed line markings for Pedestrian Crossing

Existing Surface to Road Retained

Existing Services Retained

Existing Retaining Wall Retained

Proposed secure boundary fencing offset from wall to avoid opportunities for climbing.

Existing TPO tree

Existing TPO tree

Shared Car Par: 46no. parking bays
3no. accessible parking bays

REV	DATE	DESCRIPTION
P04	28/09/2020	Trees updated to latest AIA. Issued for Planning
P03	24/09/2020	Substation Added Issued for Planning
P02	21/09/2020	Issued for Planning

CLIENT

Department for Education

CONTRACTOR

BOWMER KIRKLAND

CONSULTANT

ares LANDSCAPE ARCHITECTS

PROJECT

Wren Academy & One Degree Academy Temporary Site

DRAWING TITLE

Illustrative Plan

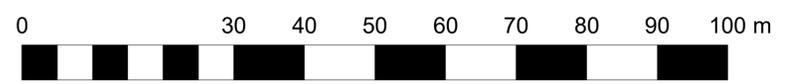
DRAWING PURPOSE

S2 - PLANNING

DRAWN BY	DATE	CHK'D BY	SCALE	MEDIA
CC	28/08/2020	CW	1:500	A1

CONTRACT DRAWING REFERENCE (AS REQUIRED)

PROJECT - ORIGINATOR - ZONE - LEVEL - TYPE - ROLE - NUMBER	REVISION
FS0404-ALA-00-XX-DR-L-0415	P04



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